

Agenda

Planning Committee

Wednesday, 1 September 2021 at 7.30 pm

New Council Chamber, Town Hall, Reigate



Meetings will take place in accordance with Government guidance. Members of the Committee will assemble at the Town Hall, Reigate.



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Members:

S. Parnall (Chairman)

M. S. Blacker
G. Adamson
J. Baker
Z. Cooper
R. Harper
A. King
F. Kelly

J. P. King
S. A. Kulka
S. McKenna
R. Michalowski
R. Ritter
C. Stevens
S. T. Walsh

Substitutes:

Conservatives:

R. Absalom, A. C. J. Horwood, J. Hudson, M. Tary and R. S. Turner

Residents Group:

R. J. Feeney, P. Harp, N. D. Harrison and C. T. H. Whinney

Green Party:

J. Booton, P. Chandler, J. C. S. Essex, S. Sinden and D. Torra

Liberal Democrats

M. Elbourne

Mari Roberts-Wood
Interim Head of Paid Service

For enquiries regarding this agenda;

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Published 23 August 2021

1. Minutes (Pages 5 - 8)

To confirm as a correct record the Minutes of the previous meeting.

2. Apologies for absence

To receive any apologies for absence.

3. Declarations of interest

To receive any declarations of interest.

4. Addendum to the agenda (To Be Tabled)

To note the addendum tabled at the meeting which provides an update on the agenda of planning applications before the Committee.

PLANNING APPLICATIONS:

NOTES:

1. The order in which the applications will be considered at the meeting may be subject to change.
2. Plans are reproduced in the agenda for reference purposes only and are not reproduced to scale. Accordingly dimensions should not be taken from these plans and the originals should be viewed for detailed information. Most drawings in the agenda have been scanned, and reproduced smaller than the original, thus affecting image quality.

To consider the following applications :

5. 20/02826/F - 19 Gatton Park Road, Redhill (Pages 9 - 46)

Demolition of the existing house and garages and construction of a two-storey residential development to provide four two-bedroom and two one-bedroom flats, along with associated car and cycle parking, waste storage and landscaped communal garden. As amended on 12/02/2021, on 22/06/2021 and on 28/07/2021.

6. 21/00546/F - 38 Alma Road, Reigate (Pages 47 - 80)

Erection of a pair of semi-detached dwellings with associated parking and landscaping following the demolition of the existing garage. As amended on 06/05/2021.

7. 21/01012/F - 42 - 44 Holmethorpe Avenue, Redhill (Pages 81 - 104)

Demolition of existing building and the redevelopment comprising 5 units within a single building and change of use from B2 general industrial use to light industrial (use class e), general industrial (B2) and storage and distribution (B8 use class) uses, together with ancillary offices, and associated parking and landscaping. As amended on 18.6.21 and 29.6.21.

8. 21/00527/F - 17 Church Road, Horley (Pages 105 - 132)

Demolition of the existing dwelling; erection of a development of six flats in a two storey building with roof accommodation together with the provision of refuse and recycling stores, 8 car parking spaces and new access. As amended on 06/08/2021.

9. 21/00924/HHOLD - 46 Raglan Road, Reigate (Pages 133 - 146)

Proposed first floor extension.

10. Development Management Quarter 1 2021-2022 Performance (Pages 147 - 150)

To inform members of the 2021/22 Q1 Development Management performance against a range of indicators.

11. Any other urgent business

To consider any item(s) which, in the opinion of the Chairman, should be considered as a matter of urgency.



Our meetings

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Notice is given of the intention to hold any part of this meeting in private for consideration of any reports containing "exempt" information, which will be marked accordingly.

BOROUGH OF REIGATE AND BANSTEAD

PLANNING COMMITTEE

Minutes of a meeting of the Planning Committee held at the New Council Chamber - Town Hall, Reigate on 7 July 2021 at 7.30 pm.

Present: Councillors S. Parnall (Chairman), M. S. Blacker (Vice-Chair), G. Adamson, J. Baker, Z. Cooper, A. King, F. Kelly, S. A. Kulka, S. McKenna, R. Ritter, C. Stevens, S. T. Walsh, J. Hudson (Substitute), R. S. Turner (Substitute) and C. T. H. Whinney (Substitute).

10. MINUTES

It was **RESOLVED** that the minutes of the meeting held on 9 June 2021 be approved as a correct record.

11. APOLOGIES FOR ABSENCE

Apologies for absence had been received from Councillors Harper, J King and Michalowski. Councillors Whinney, Hudson and Turner attended as their respective substitutes.

Councillors Blacker and Cooper attended the meeting virtually and were therefore unable to vote.

12. DECLARATIONS OF INTEREST

There were no declarations of interest.

13. ADDENDUM TO THE AGENDA

RESOLVED that the addendum be noted.

14. 21/00205/OUT - 46- 48 VICTORIA ROAD, HORLEY

The Committee considered an outline planning application at 46- 48 Victoria Road, Horley, for additional floors to accommodate 42 residential dwellings. As amended on 28/04/2021 and on 26/05/2021.

Tom Collingwood, the applicant, spoke in support of the application, stating that the intention was to develop homes and associated infrastructure which complemented the central area of Horley and modernize the existing store. No objections had been received. The revenue was needed from the residential development to secure the future of the store and its many employees. The site has had several extensions over the years. The original main apex top had to be tied back to give stability and remained fragile and if retained would require extensive work to repair which was unviable. The properties in Horley varied greatly and the proposed development would not overshadow neighbouring properties as it would be stepped back on upper levels and located on the corner and to the rear of the building is a car park. The proposal was an investment in the town and the regeneration of a key site within it.

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Reasons for approval were proposed by Councillor Stevens and seconded by Councillor Baker, whereupon the Committee voted. The original vote was tied at 6-6 with the Chairman then casting his vote to make it 7-6 against approval and 7-6 in favour of refusal. It was **RESOLVED** that planning permission be **REFUSED**.

15. 21/00413/F - 29 WOODLANDS ROAD, REDHILL

The Committee considered an application at 29, Woodlands Road, Redhill for the construction of two detached four bedroom houses including access road and parking/ landscaping. As amended on 17/06/2021.

Tim Carter, a local resident, spoke in objection to the application stating that previous planning applications at this address had been rejected by the Committee. Previously he had quoted numerous county and borough planning policies that had not been met and highlighted an objection which identified 5 contraventions of planning policy and a further 2 infringements of the National Planning Policy Framework guidelines relating to the current application; some dated back to 2006 and remained unresolved. These included the over massing of the proposed structures relative to the site; insufficient provision for amenity space; and issues relating to surface water run-off and foul water drainage. There was concern regarding safe access onto Woodlands Road. There had been 25 objections logged on the Council's website, illustrating local sentiment. Any development was inappropriate in the mature gardens of No. 29, which had previously been given a "listed" status. Any construction in its grounds would alter the immediate surroundings. It was requested whether an investigation into an extension of the existing preservation order to cover the gardens was possible to end cycle of planning applications here.

Liam Donoghue, a local resident, spoke in objection to the application stating that this was the third time the applicant had attempted to develop the site and there was little improvement since the previous application. Overall massing and the finished height of the roof ridge, reduced marginally on the plan, did not consider the complicated contours of the site. Plan drawings were misleading, and this was explained. Due to its proximity, the 10 metre plus length side wall would be just over two metres from the boundary. The height and bulk of the nearest proposed house would be overbearing. Proposed screening was inadequate. The proximity to the railway line was highlighted and there were concerns regarding the stability of the land and that trees here could be felled. Surface water management had not been addressed adequately and there were concerns of local flooding to the lower lying properties. Soakaways, as a means of storm / surface water disposal must not be constructed within 20 metres of Network Rail's boundary or at any point which could adversely affect the stability of Network Rail's property. No soil permeability survey had been completed. The roof area of both houses was immense and, when combined with the access road and parking, this would be exacerbated. The properties would be overbearing, and their size, mass, appearance and position/orientation would look out of character with the surrounding neighbourhood.

Oliver Watkins, the Agent, spoke in support of the application, stating that the erection of 2x 4 bedroom properties was in line with feedback. Height and scale had been reduced so that the new proposal had the same ridge height as those properties on St Johns Road and over 4m lower than the donor property. The development was sympathetic to the donor property and properties to the south and was situated in excess of the minimum required distances from rear to flank

elevations. The bulk and mass had been reduced by proposing two separate buildings, this gave greater amenity space to the front of the dwellings. The application was compliant with Reigate and Banstead's current DMP policy guidance. The Conservation Officer raised no objection, subject to conditions. Surrey County Council Highways, Surrey Wildlife Trust, and Reigate and Banstead's Tree officer confirmed their agreement to the proposals.

Reasons for refusal were proposed by Councillor Ritter and seconded by Councillor Whinney, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

The proposed development, by reason of the height, bulk, scale and mass of the buildings, combined with the small plot size of each dwelling and proximity to the side and rear boundaries, would constitute a cramped form of development, inconsistent with the pattern of development in locality, which would be out of keeping with and harmful to the character of the area. This would be contrary to policies DES1 and DES2 of the Reigate and Banstead Development Management Plan 2019 and advice contained within the Local Distinctiveness Design Guide SPG and the NPPF.

16. 21/00501/F - MIDDLETON HOUSE, 43-49 HIGH STREET, HORLEY

The Committee considered an application at Middleton House, 43-49 High Street, Horley for the erection of a three storey rear extension to provide ten flats (Revision of 18/01576/F to provide for an additional unit). As amended on 06/05/2021, 26/05/2021. and on 28/05/2021.

RESOLVED that the application be **APPROVED** as per recommendation and additional condition in addendum.

17. A) 21/00373/HHOLD AND B) 21/00374/LBC - 29 WEST STREET, REIGATE

The Committee considered two applications at 29 West Street, Reigate for the demolition of existing modern wooden conservatory at the rear of the property and replacement with part-single and part-two storey extensions.

Sarah Sullivan, speaking on behalf of a neighbour, spoke in objection to the application stating she was a Conservation Specialist. Concern was raised regarding privacy and scale and that the proposal did not comply with the Council's Supplementary Planning Guidance on Householder Extensions and Alterations. The ground floor extension was beyond the 45 Degree Test. However, the proposal needed to fail in the vertical plane as well, to justify refusal. In terms of the Listed Building Consent Application it also failed, as it was the duty of any proposal not to cause 'harm', but to actually enhance the Conservation Area and this proposal did not and should be refused on heritage grounds. This was overdevelopment and would give a negative contribution to the overall environment of the area. The Conservation Officer considered that other examples of flat roof extensions exist further down West Street making this application difficult to resist. All those examples pre-dated the establishment of the Conservation Area in 1973 and would not be approved now.

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Reasons for refusal were proposed by Councillor Whinney and seconded by Councillor Adamson, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposed extension would, by reason of its design, scale and form together result in a disproportionate extension to the building that would unacceptably detract from the listed building as the principal structure and would lack subservience in its appearance and be harmful to the historic character and integrity of the building. The proposal would therefore be contrary to Policies CS1 and CS4 of the Core Strategy 2014; Policies DES1, NHE9 of the Development Management Plan 2019 and guidance contained within the National Planning Policy Framework.
2. The proposed two storey extension by virtue of its size, juxtaposition and design would contribute to an unacceptable loss of amenity to the occupiers of 31 West Street through reduction of light and an overbearing relationship. It would therefore be contrary to policy CS4 of the Core Strategy, policy DES1 of the Development Management Plan 2019 and Householder Extensions and Alterations SPG 2004.

18. ANY OTHER URGENT BUSINESS

There was none.

The Meeting closed at 9.54 pm

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 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	1 st September 2021
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Hollie Marshall
	TELEPHONE:	01737 276010
	EMAIL:	Hollie.marshall@reigate-banstead.gov.uk
AGENDA ITEM:		WARD: Redhill West and Wray Common

APPLICATION NUMBER:	20/02826/F	VALID:	25 th February 2021
APPLICANT:	Gatton Park Home Ltd	AGENT:	City Planning Ltd
LOCATION:	19 GATTON PARK ROAD REDHILL SURREY RH1 2DZ		
DESCRIPTION:	Demolition of the existing house and garages and construction of a two-storey residential development to provide four two-bedroom and two one-bedroom flats, along with associated car and cycle parking, waste storage and landscaped communal garden. As amended on 12/02/2021, on 22/06/2021 and on 28/07/2021.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for demolition of the existing house and garages and construction of a two-storey residential development to provide four two-bedroom and two one-bedroom flats, along with associated car and cycle parking, waste storage and landscaped communal garden. The proposed building would accommodate three flats on the ground floor and three flats on the first floor. Nine parking spaces are proposed at the rear of the site.

The proposed building, having been significantly scaled down from the initial submission, would be of a design that complements the varied architectural styles in the vicinity and it is considered the proposed development would be acceptable in terms of its design and impact upon the character of the wider area and complies with policy DES1 of the Development Management Plan. Subject to recommended conditions, the proposal would not have a harmful impact upon the protected tree within the neighbouring property and a landscaping condition is also recommended to secure replacement tree planting within the site.

The proposal is not considered to result in a harmful impact upon the amenities of neighbouring properties subject to recommended conditions.

The proposed development would provide in excess of minimum parking standards and has been considered by the County Highways Authority who having assessed

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the application on safety, capacity and policy grounds raise no objection subject to recommended conditions.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to condition.

Neighbourhood Services: Provided comments on their requirements for refuse collection

Housing: no comments received

Sutton and East Surrey Water: no comments received

The Reigate Society: The proposed building is too large and is out of proportion for the site. The elevations look more like an office building due to the oversized windows and bulky crown roof. We have concerns about the proximity to the large tree in the neighbours land in relation to the canopy and route protection zone. We have concerns over the increase in traffic caused by the increase in residential units in this area.

Surrey Wildlife Trust: no objection subject to conditions

Representations:

Letters were sent to neighbouring properties on 11th January 2021. Neighbours were re-notified on the revised plans for a 14 day period commencing 25th June 2021 and 6th August 2021.

9 responses were received in respect of the initial plans with 3 neighbours commenting on the amended plans, raising the following issues:

Issue	Response
Overdevelopment	See paragraph 6.3 – 6.7
Inadequate parking	See paragraph 6.16
Loss of/harm to trees	See paragraph 6.21 – 6.22 and conditions 6 and 7
Loss of building	See paragraph 6.4
Increase in traffic and congestion	See paragraph 6.17
Health fears	See paragraph 6.14
Drainage/sewage capacity	See paragraph 6.29 and condition 11 and 22
Harm to wildlife habitat	See paragraph 6.26 – 6.28 conditions 8 and 9
Noise and disturbance	See paragraph 6.14 – 6.15 and condition 19

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Overbearing relationship	See paragraph 6.8 – 6.12
Overdevelopment	See paragraph 6.3 – 6.7
Overlooking and loss of privacy	See paragraph 6.8 – 6.13 and condition 24
Overshadowing	See paragraph 6.8 – 6.12
Hazard to highway safety	See paragraph 6.17
Amenity for future occupants	See paragraph 6.23 – 6.25
Loss of light	See paragraph 6.8 – 6.12
Loss of outlook	See paragraph 6.8 – 6.12
Inconvenience during construction	See paragraph 6.14 and conditions 4 and 5
Damage to neighbouring dwellings	See paragraph 6.14
Building line	See paragraph 6.4
No need for the development	Each scheme must be assessed on its own planning merits
Alternative scheme preferred	Submitted scheme must be assessed on its own planning merits
Loss of private view	Not a material planning consideration

1.0 Site and Character Appraisal

1.1 The site comprises a detached, two storey house dating from the 1950's on the southern side of Gatton Park Road. The dwelling is set back from the road with a driveway to the front and garden to the rear. To the east of the site is a detached bungalow and two storey maisonettes beyond. To the west is a three storey building of maisonettes and beyond that several detached buildings of 3 storey flats. The site therefore sits in an area of mixed character, with buildings of a variety of age, scale and design, including larger, more modern flatted blocks nearby.

1.2 To the north of the site on the opposite side of Gatton Park Road is Gatton Park, designated Historic Parks and Gardens, Metropolitan Green Belt, Area of Great Landscape Value Area of Outstanding Natural Beauty and Ancient Woodland.

2.0 Added Value

2.1 Improvements secured at the pre-application stage: The applicant entered into pre-application discussions with the Council (PAM/20/00382). Advice was provided regarding the building line, distances to side boundaries,

frontage parking, bulk scale and mass, design, access road, impact upon neighbour amenity, amenity for future occupants and impact on trees

- 2.2 Improvements secured during the course of the application: During the course of the application, amended plans have been sought to reduce the scale of the proposed development, amendments to the design, the parking layout and bin store.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|---------|---|--|
| 3.1 | 53/0155 | Erection of a detached dwellinghouse and garage on land on the south side of Gatton Park Road, Redhill, near Gatton point, and construction of vehicular access thereto | Approved with conditions
23 rd June 1953 |
| 3.2 | 70/0212 | Erection of rear extensions to lounge and dining room and front extension to garage, dwellinghouse known as The Wall House, 19 Gatton Park Road, Redhill | Approved with conditions
31 st July 1970 |

4.0 Proposal and Design Approach

- 4.1 This is a full application for demolition of the existing house and garages and construction of a two-storey residential development to provide four two-bedroom and two one-bedroom flats, along with associated car and cycle parking, waste storage and landscaped communal garden. The proposed building would accommodate three flats on the ground floor and three flats on the first floor.
- 4.2 The building would have a hipped roof design and would decrease in height towards the rear, where the land levels decrease towards the rear garden. Balconies are proposed to the front and rear elevations at first floor level. Areas of private outdoor amenity space are proposed to the rear of the site as well as an area of communal outdoor space. Landscaping is proposed to the front of the site, along with a bin store.
- 4.3 A driveway is proposed along the eastern side boundary to provide access to the rear of the site where 9 parking spaces are proposed.
- 4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:

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Assessment;
Involvement;
Evaluation; and
Design.

4.5 Evidence of the applicant's design approach is set out below:

Assessment	The character of the surrounding area is assessed as primarily composed of a mix of residential uses, including single dwellinghouses and newer flatted developments, most of which feature parking to the rear. These flatted developments include post-war buildings as well as some that were constructed more recently.
	Site features meriting retention are listed the access
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The applicant's reasons for choosing the proposal from the available options were informed by pre-application advice.

4.6 Further details of the development are as follows:

Site area	966sqm
Proposed parking spaces	9
Parking standard	8 (minimum)
Net increase in dwellings	5
Existing site density	10.35 dwellings per hectare
Proposed site density	62 dwellings per hectare
Density of the surrounding area	42 dwellings per hectare (21 – 41 Gatton Park Road 71 dwellings per hectare (Parkside Court 43 Gatton Park Road

5.0 Policy Context

5.1 Designation

Urban Area

Parking Standards – Medium Accessibility

TPO on land adjacent to the west – RE677 Wellingtonia

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS4 (Valued Townscapes and Historic Environment)
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS14 (Housing Needs)

5.3 Reigate & Banstead Development Management Plan 2019

Design, Character and Amenity (including housing) DES1, DES5, DES7, DES8,

Landscape & Nature Conservation NHE2, NHE3
Infrastructure INF1, INF3
Transport, Access and Parking TAP1,
Climate Change Resilience and CCF1, CCF2
Flooding

5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance Surrey Design
Local Distinctiveness Design Guide
Vehicle and Cycle Parking
Guidance 2018

Other Human Rights Act 1998
Community Infrastructure Levy
Regulations 2010
Conservation of Habitats and
Species Regulations 2010

6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Highway matters
- Impact on trees
- Amenity for future occupants
- Biodiversity

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- Drainage and flooding
- Sustainable construction
- Drainage
- Affordable Housing
- Community Infrastructure Levy
- Infrastructure contributions

Design appraisal

- 6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.4 The application proposes the replacement of the existing detached dwelling and garage with a new building to accommodate 6 flats. It was amended from the initial submission to reduce down in scale, losing a proposed third storey. The existing building was granted planning permission in the 1950's and is not considered to be of particular architectural merit that would warrant retention of the building. The replacement building would be two storeys in height, with a ridge height slightly lower than the existing house. The existing building line is staggered between No. 17 to the east and No's 21 – 41 to the west with No. 19 sitting slightly behind No. 17 and forward of No's 21 – 41. The proposed development would bring the building forward and the front elevation would partly follow the building line of No. 17 to the east, however the front elevation would include set back elements to both sides, stepping back twice towards the property to the west. This would break up the built form of the front elevation. The building would be set back from the front boundary by between approximately 7.5 – 9.7m retaining opportunity for landscaping, and visual separation from the street frontage so as not to appear dominating or overbearing.
- 6.5 The design of the building would complement the varied building styles and designs in the local area, incorporating large windows and a hipped roof. There would be an area of flat roof however this would be modest in scale and is not considered to harmfully detract from the appearance of the building. The rear part of the building would be set at a lower level than the front, and this part of the development would have a fully hipped roof. There is some variety in the streetscene with nearby dwellings varying in scale although there is largely a traditional style in the locality. The proposed development is considered to accord with streetscene and would integrate with the character of the area.

- 6.6 The site layout would include areas of landscaping to the front and rear parts of the site. A parking area for 9 vehicles is proposed at the rear most part of the site. This layout would be similar to nearby residential developments whereby parking is provided to the rear of the buildings.
- 6.7 Overall, it is considered the proposed development would be acceptable in terms of its design and impact upon the character of the wider area, and complies with policy DES1 of the Development Management Plan.

Neighbour amenity

- 6.8 The proposed development has been considered in terms of its impact upon neighbour amenity. To the east of the site lies No. 17 Gatton Park Road. This property is a detached bungalow. The proposed development would be set away from the side boundary with this property by 4m, with a separation of 6m between the two flank walls. Although the proposed building would extend approximately 7.8m beyond the rear elevation of No. 17, given the level of separation, the proposal is not considered to result in a harmful impact in terms of overbearing or domination. The proposal would pass the 45 degree assessment in both the horizontal and vertical plain drawn from the nearest rear and front elevation windows and is not therefore considered to result in a harmful impact in regard to loss of light.
- 6.9 No. 17 has a side facing window that looks towards the application site, historic plans show this serves a bedroom. At present this window looks towards the flank wall of the detached garage and the existing dwelling beyond. Whilst the outlook of this window would remain a flank wall, the loss of the garage would increase the separation distance to this window, and it is not considered that the resultant relationship would be so harmful as to warrant refusal of the application.
- 6.10 To the west of the site lies 21 – 41 Gatton Park Road, a building of 4 flats and 7 maisonettes. This building is sited further back within its plot than the proposed development would be, resulting in the rear elevation of the proposed development not extending beyond that of No's 21 – 41, however the front elevation would extend approximately 8.5m beyond the front of No's 21 – 41.
- 6.11 There are side facing windows in the neighbouring building, however historic plans show these serve bathrooms and therefore the proposal is not considered to result in a harmful impact in terms of outlook or loss of light. To the front of the building, the nearest windows serve a bedroom at ground floor, kitchen at first floor and bedroom at second floor. There would be a distance of 4.5m between the buildings and the proposal would not interject a 45 degree vertical plain drawn from the nearest front facing windows of the neighbouring building. Given this relationship, the proposal is not considered to result in a harmful impact in terms of loss of light, overbearing or domination.

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- 6.12 Separation distances to other nearby properties are such that the proposal is not considered to result in a harmful impact upon the amenities of other nearby residential dwellings.
- 6.13 There are balconies proposed at first floor level to serve the proposed dwellings. A condition is recommended to secure details of screening to avoid issues of overlooking and loss of privacy. Furthermore, side facing windows would be conditioned to be obscure glazed.
- 6.14 Objection has been raised from neighbouring properties regarding inconvenience during construction, noise and disturbance and health fears. Some inconvenience may occur during the construction of the proposal, however this is part and parcel of development and would not form a sustainable reason for refusal and could be mitigated by condition. Statutory nuisance legislation does however exist to control any significant harm that may occur and a construction method statement would be secured by planning condition were the application to be approved. No significant health issues are considered to arise as a result of the planning application. Damage to neighbouring dwellings during development would be a private matter between land owners.
- 6.15 The proposed development may result in some additional noise and disturbance; however, the development would be in residential use and this would not be significant enough to warrant refusal of the application. An acoustic report (by RBA Acoustics dates 11th December 2020) has been submitted with the application that shows that noise levels from the access and car park would be significantly lower than the prevailing ambient noise levels and that the resultant increase in ambient noise would be negligible. An acoustic fence is proposed along the eastern side boundary and this would be controlled by way of planning condition. Loss of a private view is not a material planning consideration.

Highway matters

- 6.16 The site is located in an area which is assessed as having a medium accessibility rating. In such areas, the Council's adopted parking standards require the provision of one space for each one and two bedroom dwelling. The application proposes a total of 9 parking spaces, one space for each dwelling and three visitor bays. The DMP requires a minimum of 8 parking spaces for a development of this size so the proposal exceeds the parking standards.
- 6.17 The proposed development has been considered by the County Highways Authority who having assessed the application on safety, capacity and policy grounds, recommends conditions to secure the site is shall not be occupied unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on the western side of the access to 19 Gatton Park Road, space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear, and the provision of fast charge sockets.

- 6.18 A condition is also recommended to secure a construction transport management plan prior to commencement of development and prior to occupation, the cycle storage is constructed, and details of the refuse collection point shall be submitted and approved.
- 6.19 The County Highways Authority note 'The sight lines from the access meet minimum requirements. The proposed parking and cycle parking meet minimum requirements. The cycle parking exceeds those minimum requirements. The developer has not shown a refuse collection point, I have asked for this to be provided via a condition requiring plans to be submitted.'
- 6.20 The submitted site layout plan shows the position of the proposed bin store adjacent to the front boundary and access road. Notwithstanding the submitted plan a condition is recommended to secure the details of the bin store.

Impact on trees

- 6.21 The neighbouring site contains mature trees that make a valued contribution to the visual amenities of the area, one protected by way of Tree Preservation Order RE677. The application was submitted with an arboricultural assessment and tree protection method statement. The Tree Officer was consulted upon the application and provided the following comments:

I have reviewed the submitted arboricultural information and the more recent revised car parking layout in respect of the above proposed development in respect of the potential impact that the development may have upon trees and vegetation, particularly the off-site trees.

The submitted arboricultural information is based upon the information, guidance and advice contained within British Standard 5837:2012 trees in relation to design, demolition and construction-Recommendations and comprises of a tree constraints plan, tree impact (more commonly known as an arboricultural assessment and tree protection method statement.

The revised information in respect of the car parking layout appears to have been designed without any input from the arboricultural consultant and now means that the submitted arboricultural information will need to be revised accordingly.

The significant trees that may suffer impact from the proposed development are located on adjoining land particularly the wellingtonia, formally protected detailed T8 and visually significant of high visual amenity and prominent in the local and wider landscape and a nearby maturing yew numbered T7 in the submitted details, both trees are categorised A and B respectively.

The off-site magnolia T4 which is lower in value and has been categorised C is still visible from public viewpoints and again is located on adjoining land.

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At section 6.1 of the report supervision and monitoring is discussed within the draft AMS would be unacceptable, levels of supervision at 'key' stages and ongoing monitoring would be expected at monthly intervals throughout any construction period and this would need to be fully addresses in the finalised AMS and TPP.

Tree protection barriers is discussed at section 6.3 of the report and at section 6.3.2.2 the use of suggested heavyweight fencing is mention and whilst only a figure 2 specification would be acceptable in these circumstances and which is referred to later in the report I am not sure why this vague description appears at this stage of the report, ground protection will be required and this will need to be robust and fit for purpose.

There is an incursion into the RPA of the wellingtonia T8 this is well within the levels of tolerance set out in the above standard; however, specialist foundation may be required and specialist surfacing with RPAs will also be required. Both of these details will need to be resolved and dealt with within the finalised AMS and a collaborative approach between the retained arboricultural consultant and the engineers to resolve these issues is expected to be documented within the finalised AMS,

Subject to supervision and monitoring which will require high levels of involvement of the retained arboricultural consultant, robust tree, and ground protection measures there should be no lasting impact or adverse effect on the future trees health of retained trees located off site.

The provision of service routings and drainage runs does not appear to be known at this stage and is not shown on the draft TPP, this is common as these engineering details will normally be prepared 'post' decision these details should also be included and shown on the finalised TPP.

The applicant, agents and constructors need to be aware that this development will require high levels of qualified arboricultural supervision and monitoring on a frequent basis as set out within these comments.

- 6.22 Conditions are recommended to secure landscaping and replacement tree planting, as well as a full tree protection condition requiring a finalised tree protection plan and arboricultural method statement. Subject to the imposition of these conditions, the development is considered acceptable in this regard.

Amenity for future occupants

- 6.23 The NPPF provides that planning decisions should provide a high standard of amenity for future users. DMP Policy DES5 relates to the delivery of high quality homes and requires, inter alia, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units except where the Council accepts that an exception to this should be made in order to provide an innovative type of affordable housing that does not meet these standards. In

addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.

6.24 The application proposes the following schedule of accommodation:

Dwelling	Internal floor area
A – 2 bed 3 person	61 sqm
B – 1 bed 1 person	38 sqm
C – 2 bed 3 person	62 sqm
D – 2 bed 3 person	61 sqm
E – 1 bed 1 person	48 sqm
F – 2 bed 3 person	61 sqm

6.25 All dwellings would meet or exceed the relevant minimum internal space standards. Each unit would have an area of outside space, whether that be a balcony or area of private outside space. The outdoor space serving flat 1b would be relatively contained and surrounded by walls on three sides with the access road to the remaining side, which is considered to provide only a limited level of amenity to this space. However, this is a one person dwelling and there is an area of communal outdoor space to the rear of the site that would benefit all future occupants. The proposal is therefore considered to comply with the requirements of policy DES5.

Biodiversity

6.26 Trees, hedges and woodland areas make a particularly valuable contribution to the character and visual amenity of the borough, both in the townscape and the landscape. They can also be valuable for biodiversity, providing important habitats for local wildlife and as part of wildlife corridors. The National Planning Policy Framework (NPPF) makes it clear (para 170) that "Planning policies and decisions should contribute to and enhance the natural and local environment by; minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures". Paragraph 174 requires the promotion of "the conservation, restoration and enhancement of priority habitats, ecological networks and the protection and recovery of priority species; and identify and pursue opportunities for securing measurable net gains for biodiversity".

6.27 During the course of the application a bat survey was submitted and this was reviewed by the Surrey Wildlife Trust (SWT). SWT commented:

The submitted report appears to be appropriate in scope and notes the likely absence of bats from the building.

The western boundary of the site was noted as being used by commuting bats. In line with recommendations within the report this should be retained as a dark corridor to enable the continued use of the site by commuting bats.

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Other recommendations regarding lighting in Table 5 of the bat report should also be adhered to.

Features suitable for roosting bats such as bat bricks or bat tiles could be incorporated into the design of the replacement building.

- 6.28 A condition is recommended to secure the development proceeds in accordance with the recommendations of the report. A condition is also recommended to secure a scheme to provide positive biodiversity benefits to provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

Drainage and flooding

- 6.29 The site is not within Flood Zones 2 or 3 and is considered to have a satisfactory impact with regard to flooding. Sewage capacity would be dealt with under Building Regulations. To ensure that the site is satisfactorily drained with regard to Development Management Plan policy CCF2 and National Planning Policy Framework 2019, a condition is recommended to secure details of a scheme for the disposal of foul and surface water drainage from the site prior to commencement of development

Sustainable construction

- 6.30 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. Whilst the applicant has not acknowledged the updated requirements of the DMP, in the event that planning permission is to be granted, a condition could be imposed to seek such information and its implementation prior to the first occupation of development. In this regard, there would be no conflict with DMP Policy CCF1.
- 6.31 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.
- 6.32 Policy DES7 of the DMP requires that on sites of 5 or more homes at least 20% of homes should meet the Building Regulations requirements for 'accessible and adaptable dwellings'. The applicant has not referred to this requirement. Without any evidence to the contrary it is considered that such a requirement would be viable for the applicant and therefore a condition is recommended to secure adequate accessible housing in accordance with policy DES7.

Affordable Housing

- 6.33 Core Strategy Policy CS15 requires financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, the Government removed the ability to require such contributions from small scale developments in 2014 and therefore no such requirement is made for this site, in accordance with the more recent Development Management plan and Affordable Housing SPD.

Community Infrastructure Levy (CIL)

- 6.34 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and, although the exact amount would be determined and collected after the grant of planning permission, an informal calculation shows a CIL liability of around £16,173.

Infrastructure Contributions

- 6.35 In terms of other contributions and planning obligations, The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified. Accordingly, any request for an infrastructure contribution would be contrary to CIL Regulation 122.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Existing Plans	19		16.12.2020
Site Layout Plan	CP/2020/36/09		12.02.2021
Site Layout Plan	CP/2020/36/10		12.02.2021
Proposed Plans	13		22.06.2021
Elevation Plan	12		22.06.2021

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Elevation Plan	11	22.06.2021
Elevation Plan	10	22.06.2021
Floor Plan	9	22.06.2021
Floor Plan	8	22.06.2021
Proposed Plans	6	22.06.2021
Elevation Plan	5	22.06.2021
Elevation Plan	4	22.06.2021
Existing Plans	3	22.06.2021
Floor Plan	2	22.06.2021
Location Plan	1	22.06.2021
Proposed Plans	20	22.06.2021
Proposed Plans	2	28.07.2021
Site Layout Plan	7	28.07.2021
Proposed Plans	1	28.07.2021
Proposed Plans	17	28.07.2021
Landscaping Plan	15	28.07.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (j) on-site turning for construction vehicleshas been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019, Reigate and Banstead Core Strategy 2014 Policy CS17 and Reigate and Banstead Development Management Plan September 2019 policies TAP1 and DES8

5. No development shall commence until a Construction Management Statement, to include details of:
- Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
 - Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
 - Means of communication and liaison with neighbouring residents and businesses.
 - Hours of work.

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

6. No development shall commence including demolition and or any groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalised Arboricultural Method Statement (AMS) shall be compiled in conjunction with the construction method statement is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings, drainage runs and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & ongoing monitoring with an agreed direct reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Local Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

7. No development shall commence on site until a scheme for the landscaping and replacement tree planting of the site including the retention of existing landscape features has been submitted and approved in writing by the local planning authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass

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establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or use of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Borough Local Plan 2019, British Standards including BS8545:2014 and British Standard 5837:2012

8. The development shall be carried out in accordance with mitigation measures set out within table 5 of the Bat Emergence and Re-entry report dated 14th May 2021.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

9. No development above ground level shall commence until a scheme to provide positive biodiversity benefits has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2

10. Prior to commencement of development, details of external lighting, including proposed operating times and details of shielding to control light spill, shall be submitted to and approved, in writing, by the Planning Authority, and there shall be no variance in external lighting other than as approved.

Reason: In the interests of the amenities of neighbouring residential properties and to ensure the protection of the protected species with regard to Development Management Plan 2019 policy NHE2 and Reigate and Banstead Core Strategy 2014 policy CS2, the NPPF and the Natural Environment and Rural Communities Act (2006).

11. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:

- a) Evidence that there is no risk of contamination through the infiltration SuDs
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development (Pre, Post and during), associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 37.8l/s.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element including details of any flow restrictions and maintenance / risk reducing features (silt traps, inspections chambers etc)
- d) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational
- e) Details of drainage management responsibilities and maintenance regimes for the drainage system
- f) A plan showing exceedance flows (ie during rainfall greater than design events or during blockage) and how property on and offsite will be protected. This should include details of how surface water run-off entering the site from the bunded northern boundary will be intercepted.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

12. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

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Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

13. No development above slab level shall take place until details setting out how the applicant will ensure that at least 20%, unless otherwise agreed in writing, of the homes meet the Building Regulations requirements for 'accessible and adaptable dwellings' have been submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed details.

Reason: In order that the scheme provides accessible housing in accordance with Reigate and Banstead Development Management Plan 2019 policy DES7.

14. The development hereby approved shall not be first occupied unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on the western side of the access to 19 Gatton Park Road, the depth measured from the back of the footway and the width outwards from the edge of the access. No obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

15. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved site plan dated 10 06 21 for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the car parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access, and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

16. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved cycle secure storage plan and site plan both dated 10 06 21 for 12 bicycles to be stored in a sheltered and accessible location. Thereafter the bicycle parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework

2021 and Policy TAP1 Parking, access , and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

17. The development hereby approved shall not be occupied unless and until a minimum of two of the proposed parking spaces are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) and another minimum of two of the parking spaces are provided with an electrical supply to retrospectively fit an electric vehicle charging point in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

18. The development shall not be occupied until a refuse collection point has been provided within 25 metres of the highway in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority, all to be permanently retained.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

19. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected, including the acoustic fencing along the eastern side boundary, has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3

20. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day

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- b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

- 21. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
 - a) A broadband connection accessed directly from the nearest exchange or cabinet
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

- 22. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

- 23. The ground and first floor windows in the east and west side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

24. The development shall not be used or occupied until such time as details of physical screening of the balconies have been submitted to and approved in writing by the local planning authority and completed in full accordance with the approved scheme. The screening shall be maintained thereafter.

Reason: To ensure no harm occurs to the amenity of neighbouring properties with regards to policy DES1 of the Development Management Plan

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. The Council's Neighbourhood Services team can be contacted on 01737 276292 or via the Council's website at http://www.reigate-banstead.gov.uk/info/20085/planning_applications/147/recycling_and_waste_developers_guidance
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and

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(g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering
7. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover or to install dropped kerbs. Please see www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs.
8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned

wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

9. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
10. A pedestrian inter-visibility splay of 2m by 2m shall be provided on the west side of the access, the depth measured from the back of the footway and the widths outwards from the edges of the access. No fence, wall or other obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays. The wall along the site frontage needs to be reduced in height to 0.6 metres high above the ground from the lowest point of the site at a point 2 metres back into the site from the carriageway edge.
11. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
12. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above landscaping condition. The planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
13. Biodiversity enhancements – with regard to condition 9 the Council expects the applicant to provide an appropriately detailed document to demonstrate that a measurable net gain (not just compensation), secure for the life time of the development, is achievable. The applicant may wish to use an appropriate metric such as the DEFRA Biodiversity Metric 2.0 to demonstrate how the site will provide biodiversity net gain. If net gain cannot be met this must be fully justified.
14. The applicant is advised that the development should seek to achieve standards contained within the Secured by Design award scheme to ensure that it results in a safe development.

Agenda Item 5

Planning Committee
1st September 2021

Agenda Item: 5
20/02826/F

REASON FOR PERMISSION

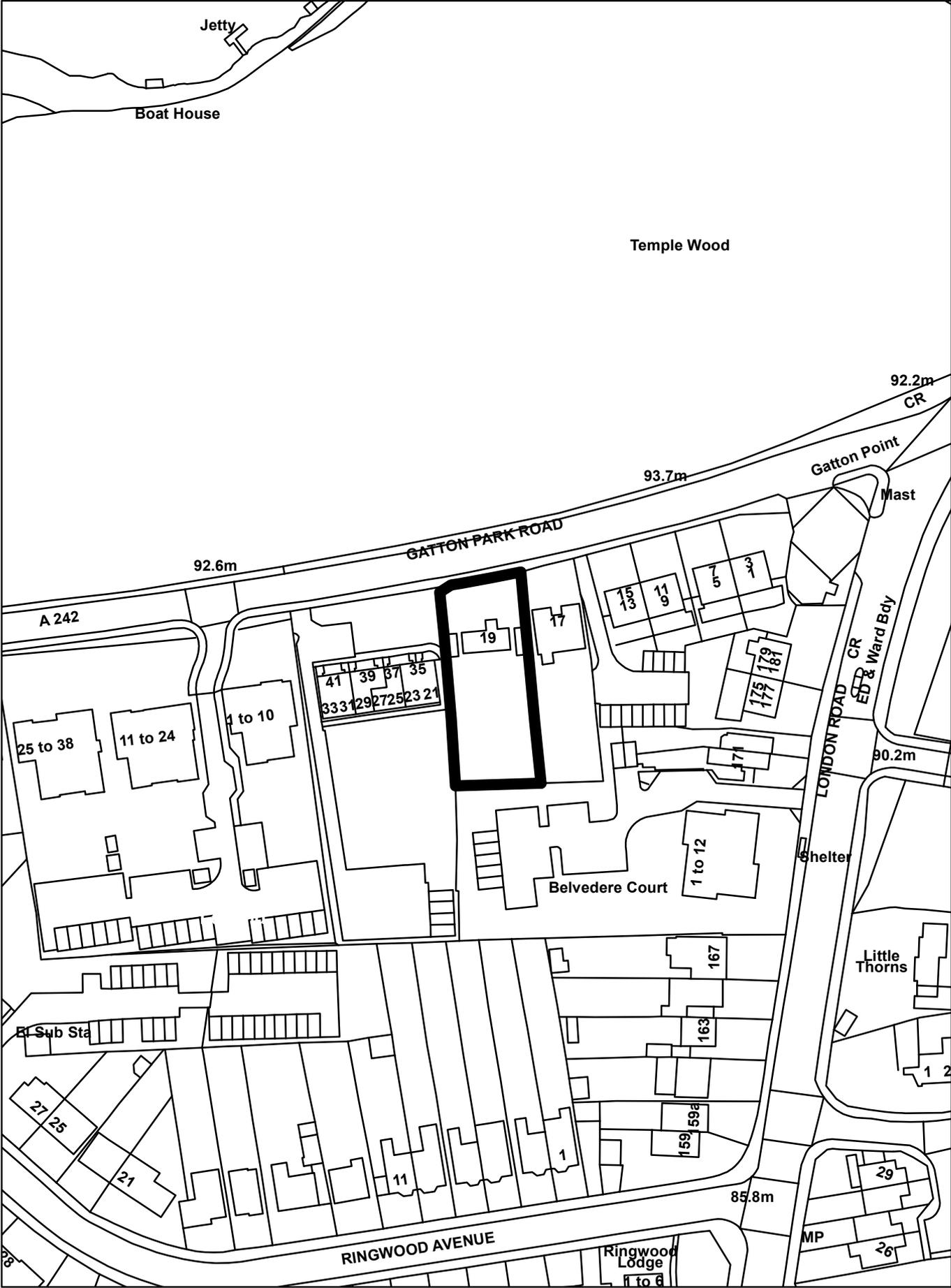
The development hereby permitted has been assessed against development plan policies CS1, CS4, CS10, CS11, CS14, DES1, DES5, DES7, DES8, TAP1, CCF1, CCF2, NHE2, NHE3, INF3, TAP1, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

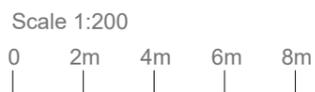
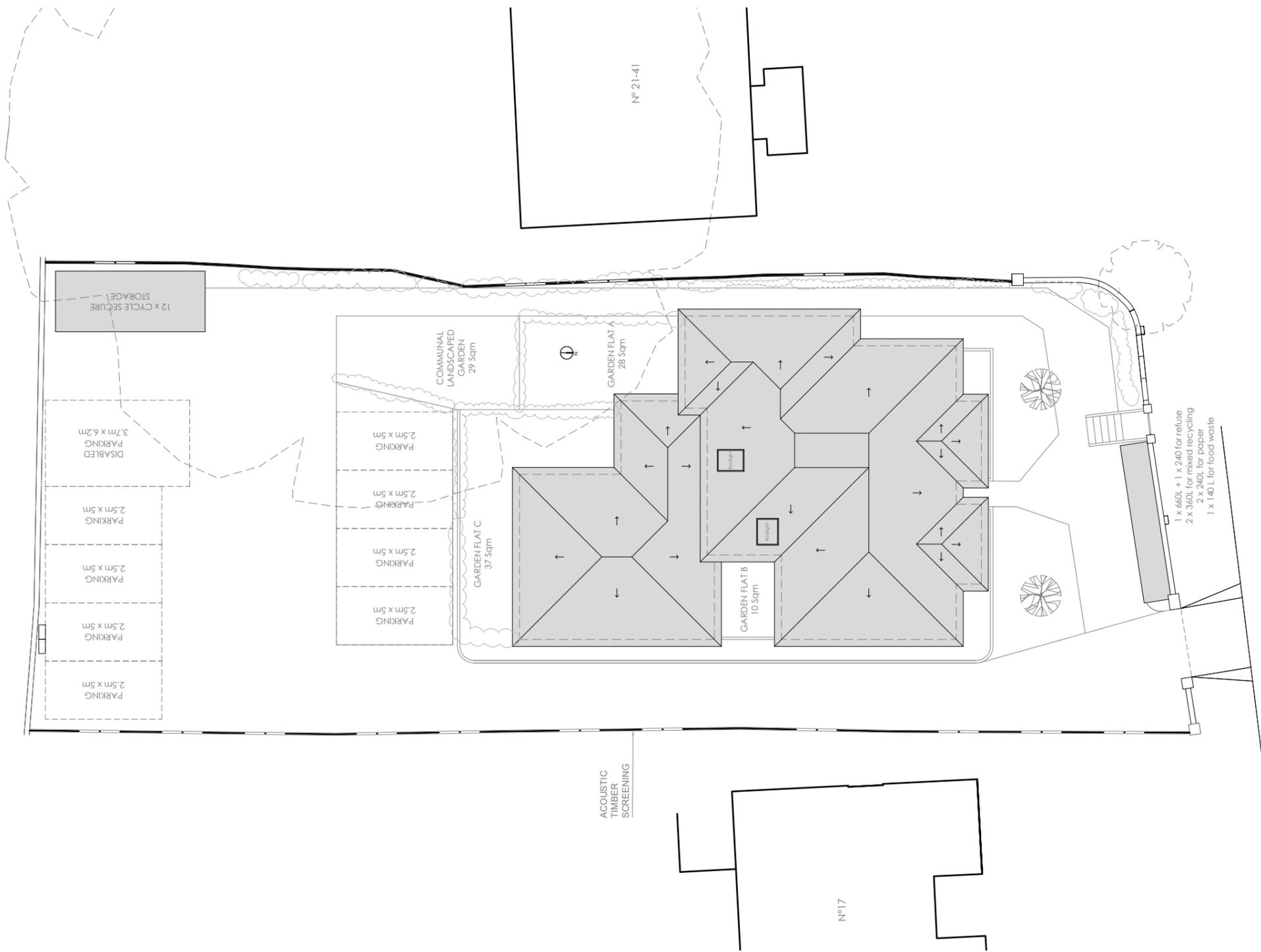
Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

20/02826/F

19 Gatton Park Road Redhill Surrey RH1 2DZ





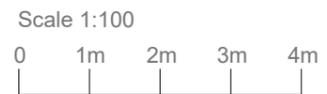
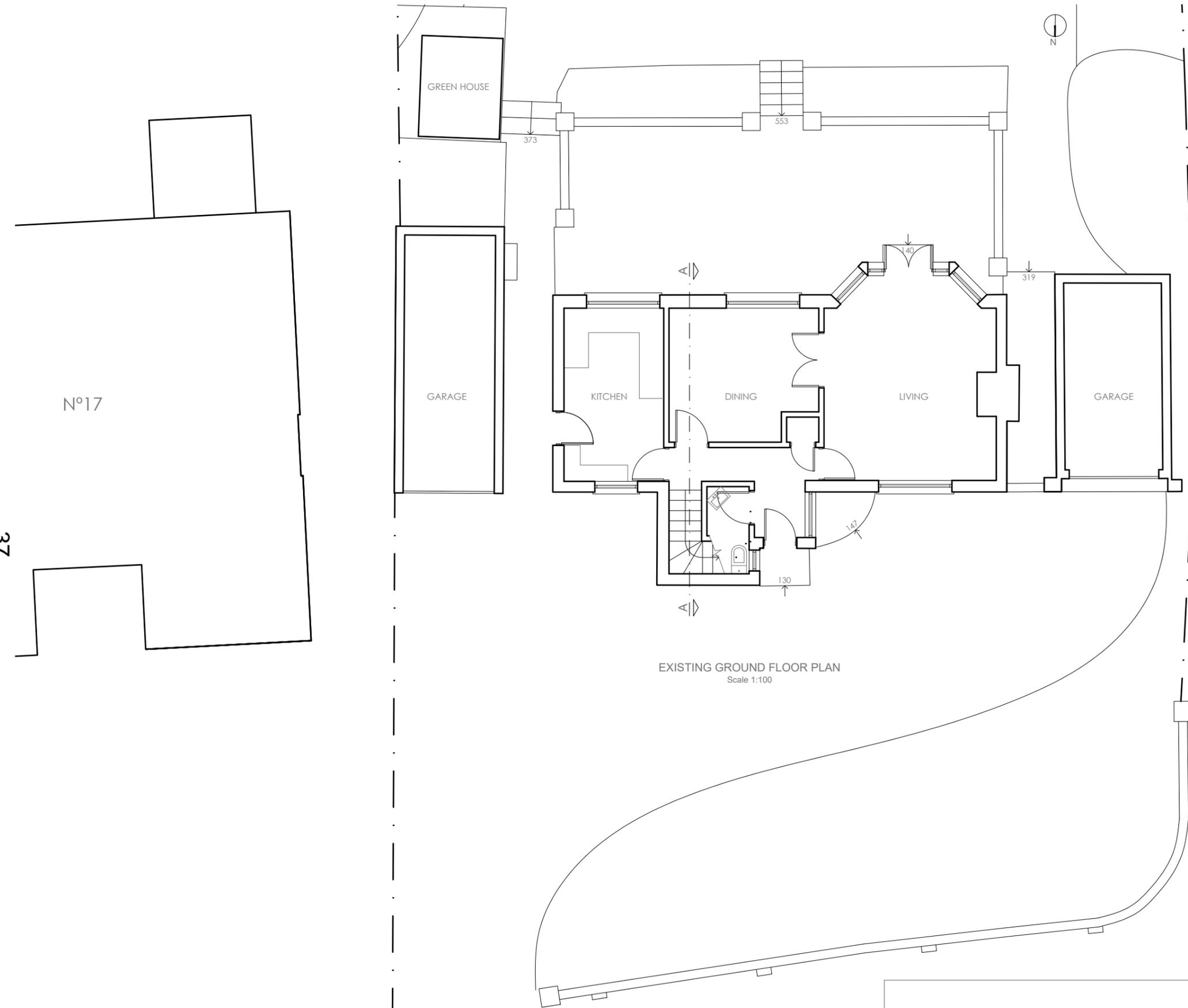
NOTE: Survey information regarding N°s 17 and 21 to 41 of Gatton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

**PROPOSED
SITE PLAN**

DATE	22-07-21
FILE	19 Gatton Park Road_22-07-21_01
LAYOUT	7
SCALE	1:200

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS DO NOT SCALE FROM THIS DRAWING CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY



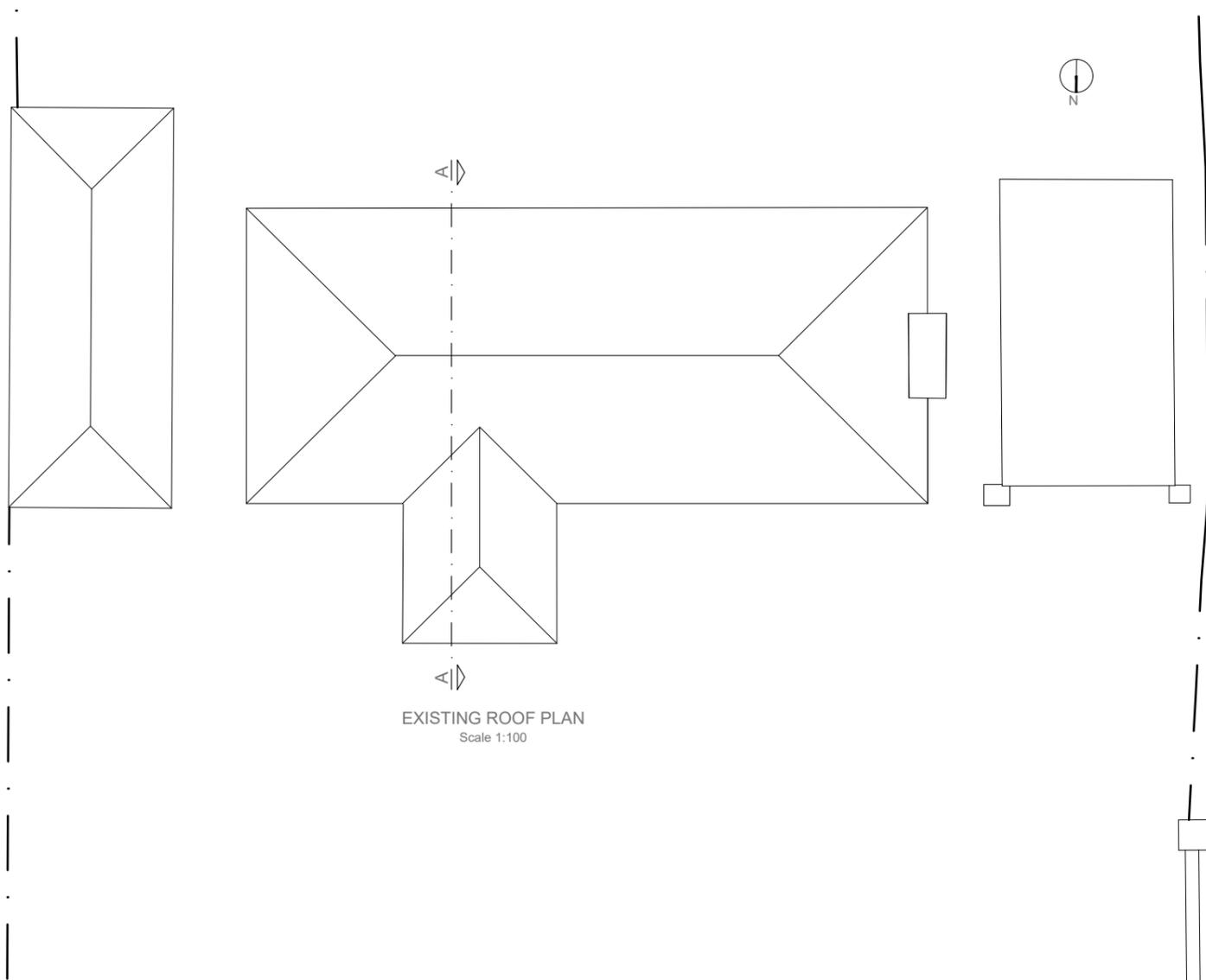
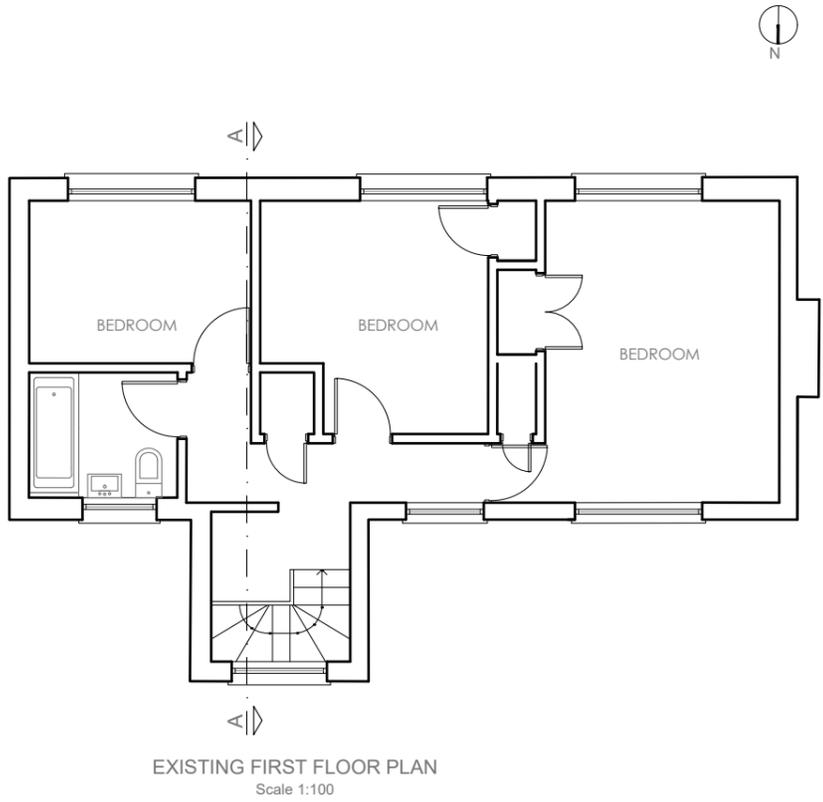
NOTE: Survey information regarding N°s 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

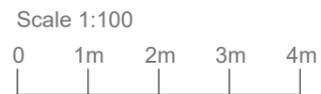
**EXISTING
PLANS**

DATE	10-06-21
FILE	19 Gattton Park Road_10-06-21_01
LAYOUT	2
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS DO NOT SCALE FROM THIS DRAWING CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY



38



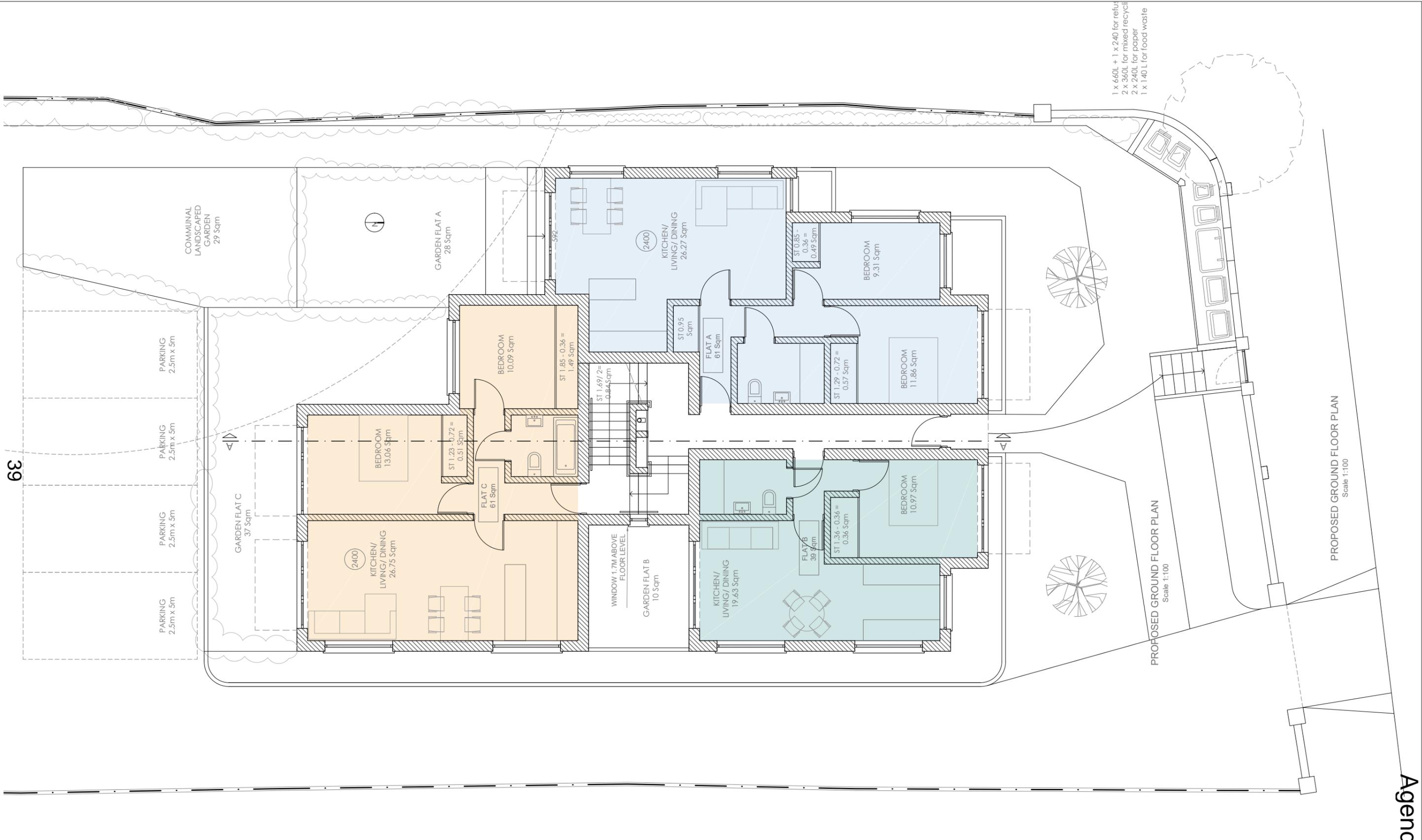
NOTE: Survey information regarding N^os 17 and 21 to 41 of Gatton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

**EXISTING
PLANS**

DATE	10-06-21
FILE	19 Gatton Park Road_10-06-21_01
LAYOUT	3
SCALE	1:100

**DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS
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CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE
PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY**

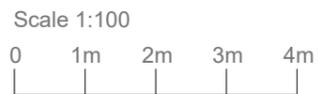


NOTE: Survey information regarding N^os 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD RH1 2DZ, REDHILL, SURREY PROPOSED PLANS	DATE	10-06-21
	FILE	19 Gattton Park Road_10-06-21_01
	LAYOUT	8
	SCALE	1:100
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PROPOSED FIRST FLOOR PLAN
Scale 1:100



NOTE: Survey information regarding N^os 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

**PROPOSED
PLANS**

DATE	10-06-21
FILE	19 Gattton Park Road_10-06-21_01
LAYOUT	9
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS
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CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE
PLANT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

N° 21-41

N°17

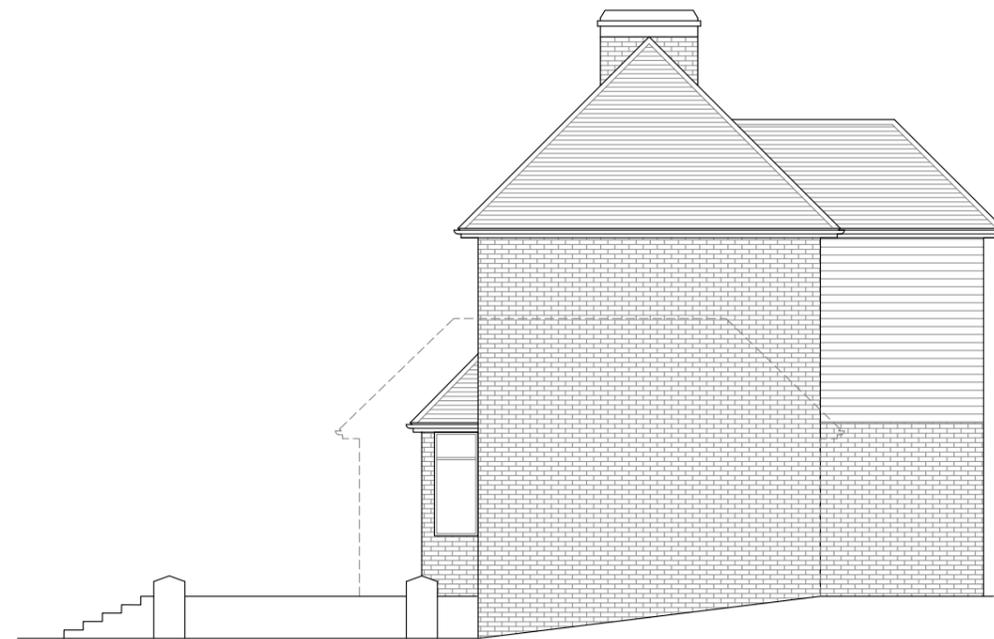


EXISTING NORTH ELEVATION

41



EXISTING WEST ELEVATION



EXISTING EAST ELEVATION

Scale 1:100



NOTE: Survey information regarding N°s 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

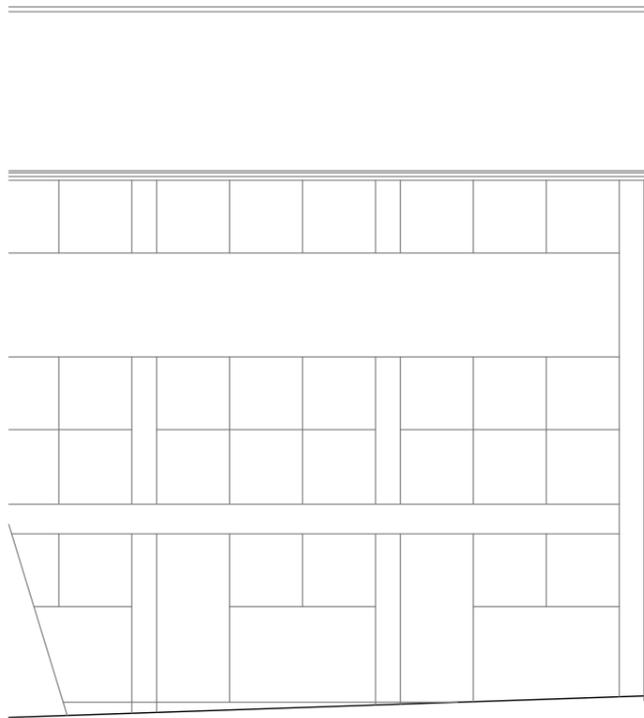
19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

EXISTING
FRONT AND SIDE ELEVATIONS

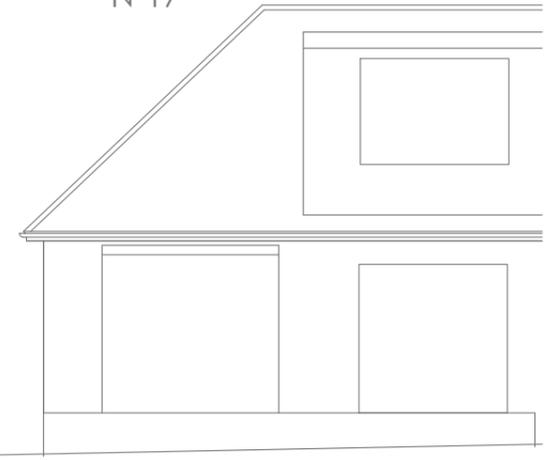
DATE	10-06-21
FILE	19 Gattton Park Road_10-06-21_01
LAYOUT	4
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS DO NOT SCALE FROM THIS DRAWING CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

N° 21-41

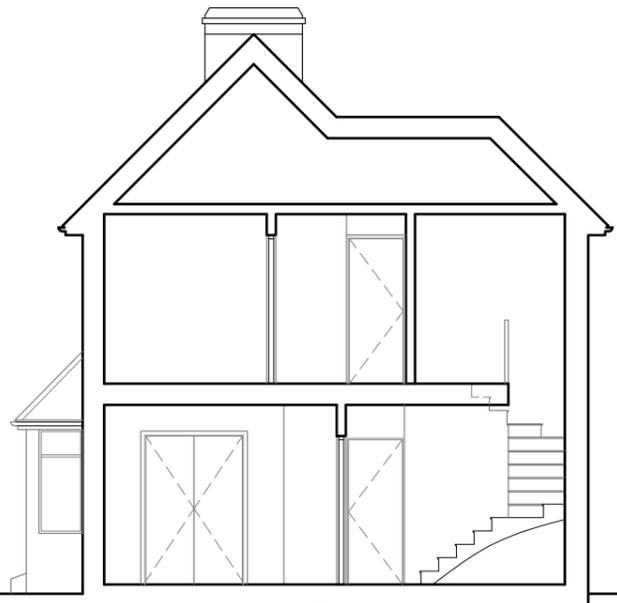


N°17



EXISTING SOUTH ELEVATION

42



EXISTING SECTION A-A

Scale 1:100



NOTE: Survey information regarding N°s 17 and 21 to 41 of Gatton Park Road has been taken from ordinance survey maps, aerial images and photography

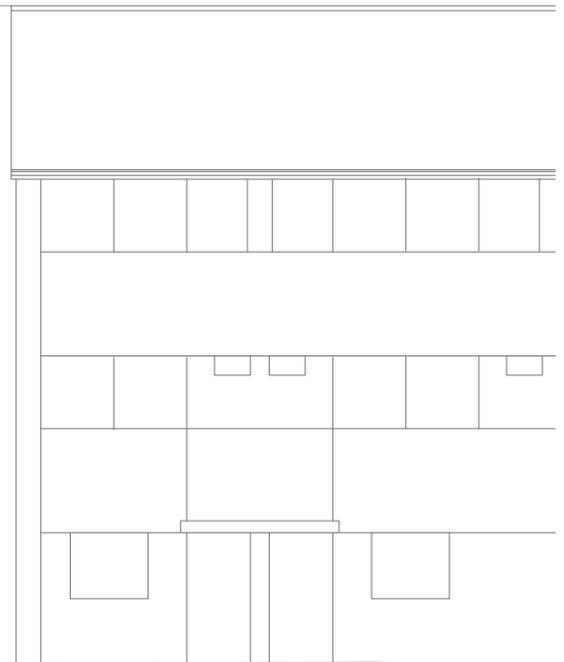
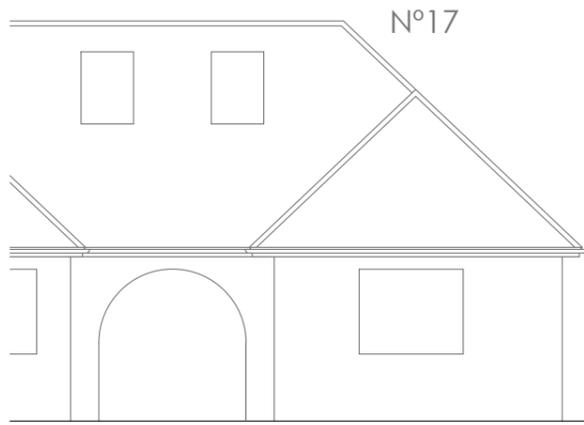
19 GATTON PARK ROAD

RH1 2DZ, REDHILL, SURREY

**EXISTING
REAR ELEVATION - SECTION**

DATE	10-06-21
FILE	19 Gatton Park Road_10-06-21_01
LAYOUT	5
SCALE	1:100

**DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS
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CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE
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PROPOSED NORTH ELEVATION

43

ELEVATIONS KEY

-  FACING BRICKWORK (YELLOW)
-  FACING BRICKWORK (RED)
-  WHITE COLOUR RENDER
-  CERAMIC TILES ROOF
-  PAINTED HARDWOOD WINDOW/ DOOR
CLEAR GLAZING
-  OBSCURED GLAZING SCREEN
-  GLAZING BALUSTRADE
-  UPVC GUTTERING



PROPOSED STREET ELEVATION



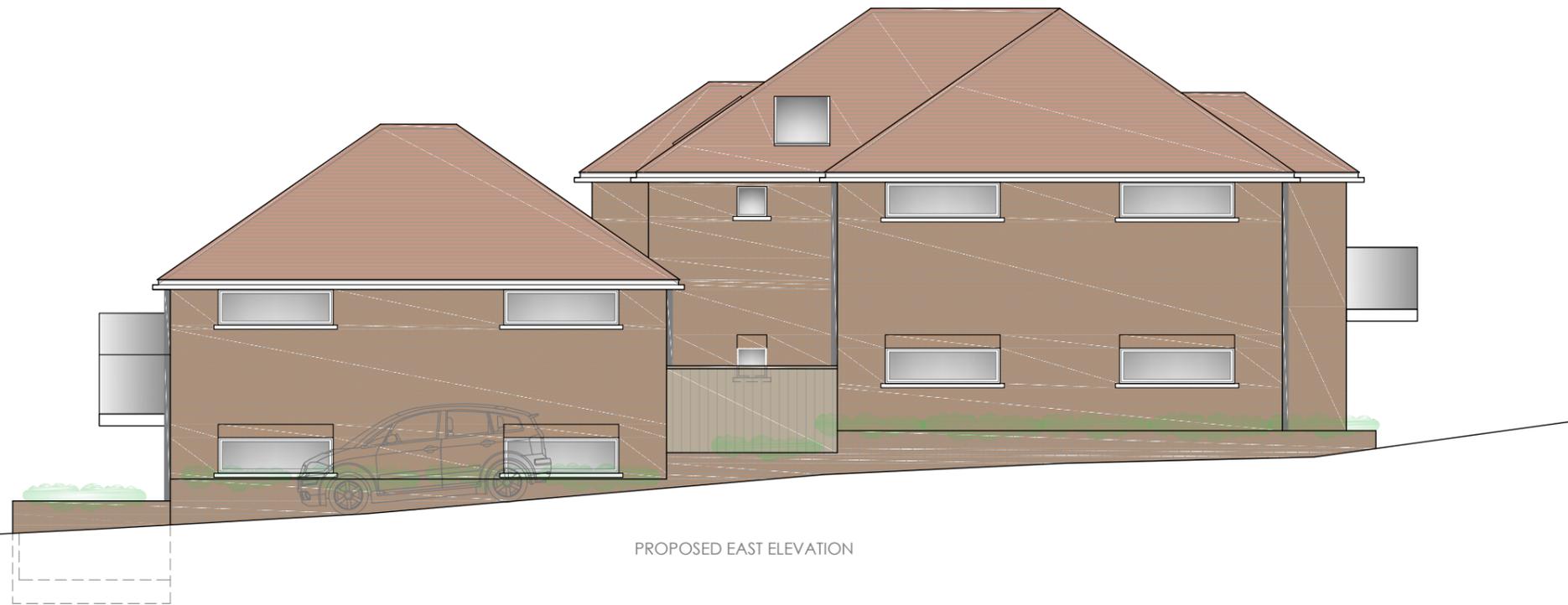
NOTE: Survey information regarding N°s 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

**PROPOSED
FRONT AND STREET ELEVATIONS**

DATE	10-06-21
FILE	19 Gattton Park Road_10-06-21_01
LAYOUT	10
SCALE	1:100

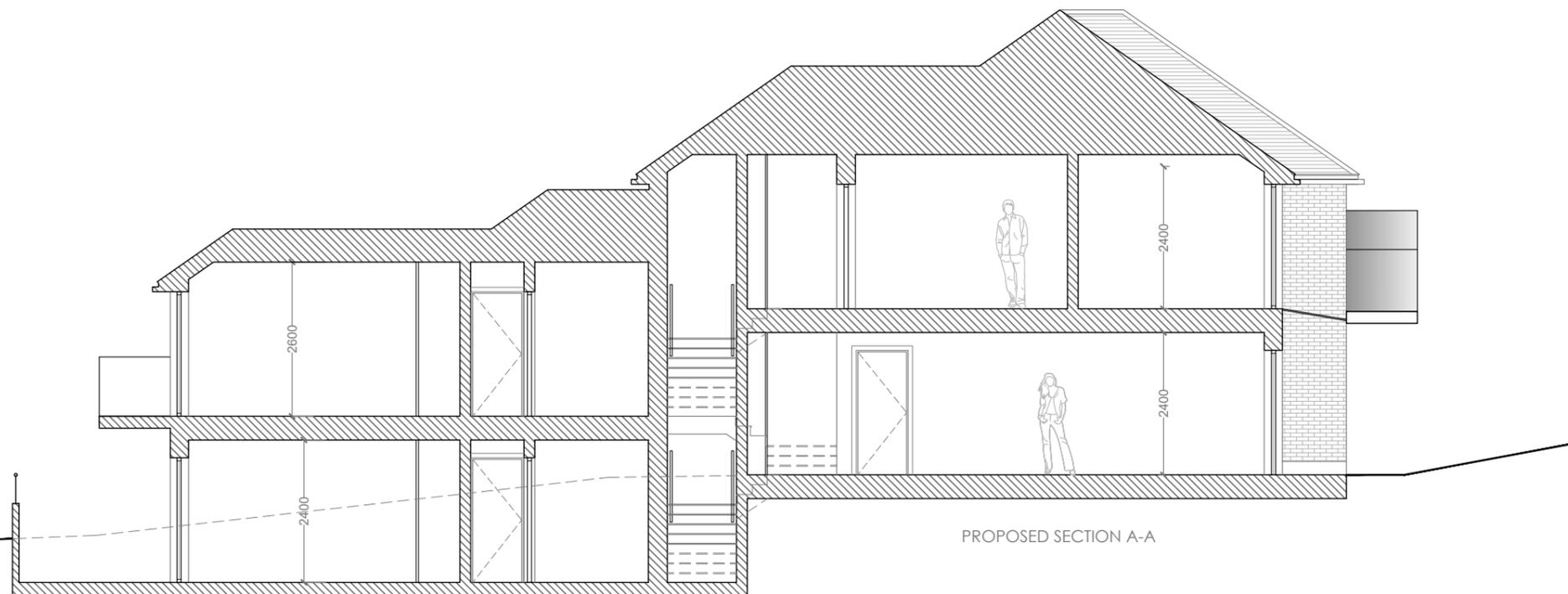
DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS
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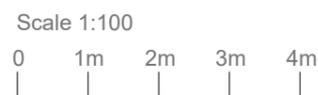
- ELEVATIONS KEY**
- FACING BRICKWORK (YELLOW)
 - FACING BRICKWORK (RED)
 - WHITE COLOUR RENDER
 - CERAMIC TILES ROOF
 - PAINTED HARDWOOD WINDOW/ DOOR
CLEAR GLAZING
 - OBSCURED GLAZING SCREEN
 - GLAZING BALUSTRADE
 - UPVC GUTTERING

PROPOSED EAST ELEVATION

44



PROPOSED SECTION A-A



NOTE: Survey information regarding N^os 17 and 21 to 41 of Gatton Park Road has been taken from ordinance survey maps, aerial images and photography

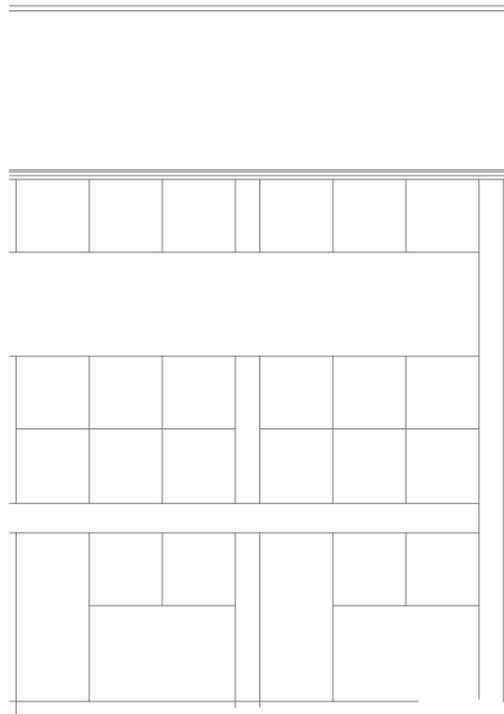
19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

PROPOSED
SIDE ELEVATION - SECTION

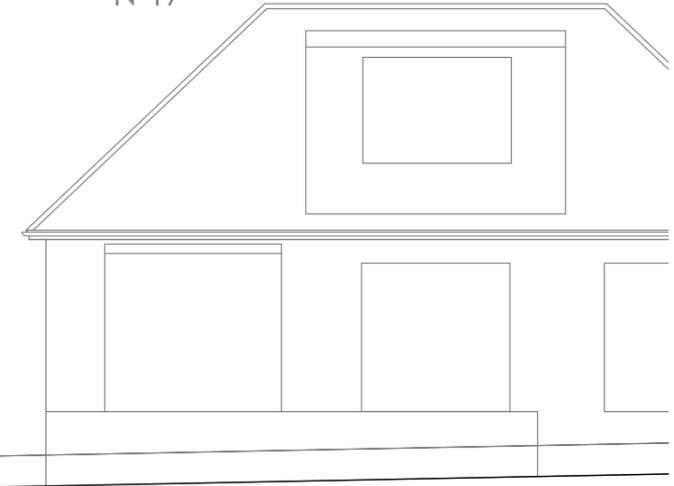
DATE	10-06-21
FILE	19 Gatton Park Road_10-06-21_01
LAYOUT	12
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS DO NOT SCALE FROM THIS DRAWING CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

N° 21-41

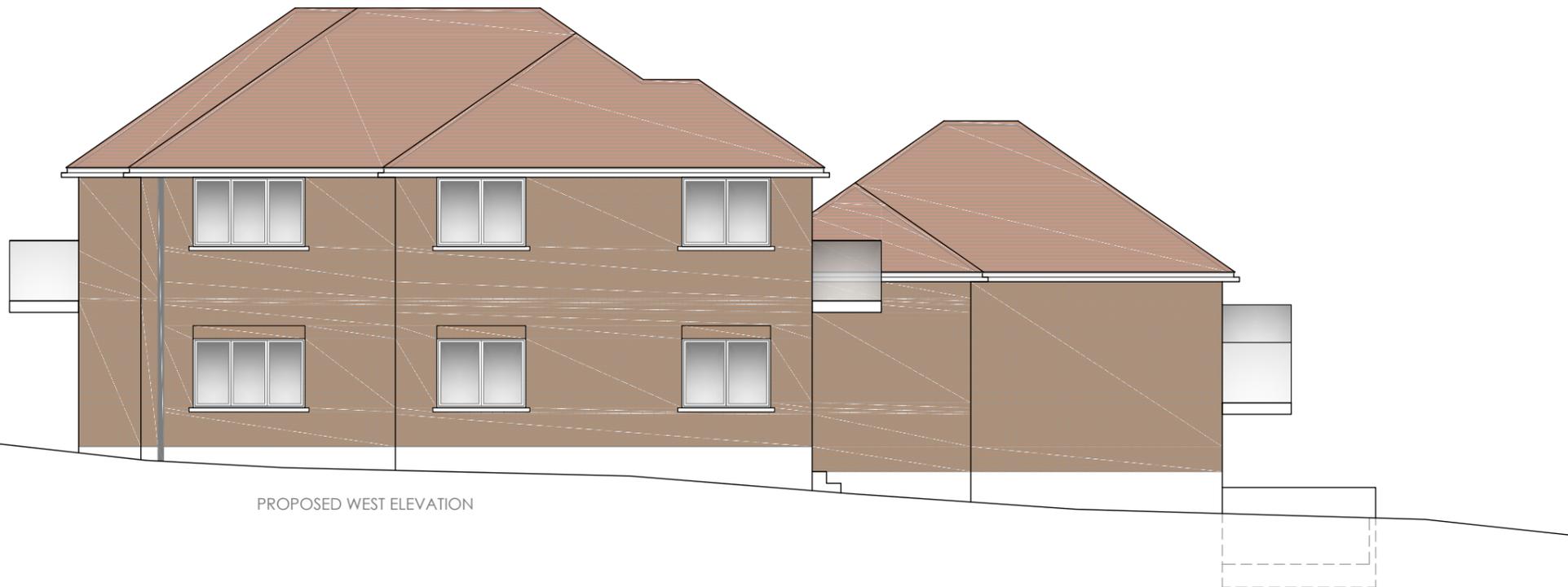


N°17



PROPOSED SOUTH ELEVATION

45



PROPOSED WEST ELEVATION

ELEVATIONS KEY

-  FACING BRICKWORK (YELLOW)
-  FACING BRICKWORK (RED)
-  WHITE COLOUR RENDER
-  CERAMIC TILES ROOF
-  PAINTED HARDWOOD WINDOW/ DOOR
CLEAR GLAZING
-  OBSCURED GLAZING SCREEN
-  GLAZING BALUSTRADE
-  UPVC GUTTERING

Scale 1:100



NOTE: Survey information regarding N°s 17 and 21 to 41 of Gattton Park Road has been taken from ordinance survey maps, aerial images and photography

19 GATTON PARK ROAD
RH1 2DZ, REDHILL, SURREY

**PROPOSED
REAR AND SIDE ELEVATIONS**

DATE	10-06-21
FILE	19 Gattton Park Road_10-06-21_01
LAYOUT	11
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS DO NOT SCALE FROM THIS DRAWING CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

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Agenda Item 6

Planning Committee
1st September 2021

Agenda Item: 6
21/00546/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	1 st September 2021
	REPORT OF:	HEAD OF PLACES & PLANNING
	AUTHOR:	Hollie Marshall
	TELEPHONE:	01737 276010
	EMAIL:	hollie.marshall@reigate-banstead.gov.uk
AGENDA ITEM:	6	WARD: Reigate

APPLICATION NUMBER:	21/00546/F	VALID:	25 March 2021
APPLICANT:	Emmerton Developments	AGENT:	WS Planning & Architecture
LOCATION:	38 ALMA ROAD REIGATE SURREY RH2 0DH		
DESCRIPTION:	Erection of a pair of semi-detached dwellings with associated parking and landscaping following the demolition of the existing garage. As amended on 06/05/2021.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application for the erection of a pair of semi-detached dwellings with associated parking and landscaping following the demolition of the existing garage. The proposed new dwellings would be sited at the rear most part of the garden of 38 Alma Road with access to the dwellings to the south side of the donor dwelling. The new access road would head northward into the site and provide a driveway to the front of both dwellings.

The proposed semi-detached houses would be two storeys with accommodation in the roof space and the design would be traditional in style. The dwellings would follow the contours and changing land levels within the site, with the dwelling to the northern side of the site occupying a slighter higher level than the dwelling on the southern side. Two parking spaces would be provided to the front of each dwelling, with one dwelling would also have an attached single garage.

The application follows a recent appeal for two detached dwellings on the site that was dismissed. In dismissing the appeal, the Inspector found harm as a result of the massing of the proposal and its relationship to the host plot size, the character of the RASC and the loss of spacing in this location. This application seeks to overcome these issues by proposing a pair of semi-detached houses. The layout would afford significantly increased distances to side boundaries and the rear boundary of the site. Furthermore, both dwellings would have either a single, or one and a half storey element on the north or south side, further increasing the visual separation at first floor level. A detached garage was also previously proposed in the refused

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scheme in the north eastern corner of the site, this has been removed. The cumulation of these changes are considered to result in a more spacious layout and one which would accord with the pattern of development in the wider area and RASC.

The proposal would meet the Council's parking standards as set out within the Development Management Plan. The County Highways Authority have raised no objection to the proposal subject to recommended conditions.

The proposal is not considered to result in a harmful impact upon neighbour amenity.

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions.

Surrey Wildlife Trust: no objection subject to recommended conditions

Drainage Consultant: no objection subject to recommended conditions

Representations:

Letters were sent to neighbouring properties on 31st March 2021.

10 neighbours have submitted representations raising the following issues:

Issue	Response
Loss of a private view	See paragraph 6.19
Out of character with surrounding area	See paragraph 6.3 – 6.12
Dominating	See paragraph 6.13 – 6.17
Overbearing relationship	See paragraph 6.13 – 6.17
Cramped	See paragraph 6.3 – 6.12
Loss of/harm to trees	See paragraph 6.23 – 6.24 and condition 6
Flooding	See paragraph 6.30 and condition 8
Harm to RASC	See paragraph 6.3 – 6.12
Increase in traffic and congestion	See paragraph 6.21
Inadequate parking	See paragraph 6.20
Hazard to highway safety	See paragraph 6.21
Inconvenience during construction	See paragraph 6.18 and condition 4
No need for the development	See paragraph 6.1
Overdevelopment	See paragraph 6.3 – 6.12
Overlooking and loss of privacy	See paragraph 6.13 – 6.17
Noise and disturbance	See paragraph 6.19
Poor design	See paragraph 6.9
Harm to wildlife habitat	See paragraph 6.27 – 6.29 and conditions 5 and 17

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Alternative location/proposal preferred

See paragraph 6.3 – 6.12

Harm to Conservation Area

See paragraph 6.11

1.0 Site and Character Appraisal

- 1.1 The site comprises part of the rear gardens of a semi-detached dwelling which is set in a generous, irregular shaped plot on the north western side of Alma Road. The site is relatively flat in parts towards the rear and undulating in others. The site occupies a wide frontage in the street scene before narrowing slightly and then increasing in width towards the rear. The irregular shape of the plot is not characteristic of the RASC where the predominant pattern of development is frontage dwellings in a linear form, with infill development at Sheridan Drive and Beverley Heights which align more closely with the siting of the proposed development.
- 1.2 The site is within the Alma Road and Raglan Road Residential Area of Special Character which is characterised by dwellings set in large, spacious plots, with a predominance of tree cover, and part of the site is covered by a Tree Preservation Order.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was sought for two detached dwellings, concern was expressed over scale of development and it was considered that a reduction in the bulk, scale and mass of the dwellings would be required, a reduction in the level of hardstanding, reduction in spread of development across the site and an increase in soft landscaping in order to address the previous reasons for refusal and issues raised in the recent appeal decision
- 2.2 Improvements secured during the course of the application: During the course of the application an ecology report was sought and a plan to show the required visibility splays. Both were submitted by the Applicant. Furthermore, amendments have been sought to remove the side facing dormer window to the northern elevation.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|--------------|--|---|
| 3.1 | 92/09100/OUT | Erection of 5 bedroom detached dwelling | Refused
11 November 1992 |
| 3.2 | 18/00469/F | Erection of two detached dwellings with associated parking and landscaping following the demolition of existing garage | Refused
24 April 2018
Appeal dismissed
20 March 2019 |

3.3 Appeal decision 18/00469/F/AP is appended to this report.

4.0 Proposal and Design Approach

4.1 This is a full application for the erection of a pair of semi-detached dwellings with associated parking and landscaping following the demolition of the existing garage. The proposed new dwellings would be sited at the rear most part of the garden of 38 Alma Road with access to the dwellings to the south side of the donor dwelling. The new access road would head northward into the site and provide a driveway to the front of both dwellings.

4.2 The proposed semi-detached houses would be two storeys with accommodation in the roof space and the design would be traditional in style. The dwellings would follow the contours and changing land levels within the site, with the dwelling to the northern side of the site occupying a slighter higher level than the dwelling on the southern side.

4.3 Two parking spaces would be provided to the front of each dwelling along with turning space so vehicles may enter and leave the site in forward gear. One dwelling would also have an attached single garage.

4.4 The dwellings would have rear gardens of between 14 – 18m in depth along with amenity space to the side of the dwellings between approximately 10 – 12m.

4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.6 Evidence of the applicant’s design approach is set out below:

Assessment	The character of the surrounding area is assessed as predominantly residential in character and comprises a mix of detached and semi-detached dwellings. Land to the north, east and west of the application site comprises residential development. Land to the south comprises residential development, Holmesdale Community Infant School and Reigate Town Centre.
	Site features meriting retention are listed as existing trees where possible
Involvement	No community consultation took place.

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Evaluation	The other development options considered were two detached dwellings.
Design	The applicant's reasons for choosing the proposal from the available options were informed by a pre-application enquiry in 2019 (LPA Ref: PAM/19/00206)

4.7 Further details of the development are as follows:

Site area	0.2654 hectares
Proposed parking spaces	5
Parking standard	4 (minimum)
Net increase in dwellings	2
Existing site density	2.3 dwellings per hectare
Proposed site density	7 dwellings per hectare
Density of the surrounding area	4.6 dwellings per hectare (32 – 40 even nos) 5.1 dwellings per hectare (27 – 35 odd nos) 7.4 dwellings per hectare (Sheridan Drive)

5.0 Policy Context

5.1 Designation

Urban Area
Alma Road and Raglan Road Residential Area of Special Character
Tree Preservation Order RE1418A - 1 Ash, 1 Oak and 3 Yew
Parking Standards – Medium accessibility
Surface Water Flooding – 1 in 30 years
Surface Water Flooding – 1 in 1000 years

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS2 (Valued Landscapes and Natural Environment),
CS4 (Valued Townscapes and Historic Environment)
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS14 (Housing Needs)
CS15 (Affordable Housing)

5.3 Reigate & Banstead Development Management Plan 2019

DES1 (Design of New development)

DES2 (Residential garden land development)
DES3 Residential Areas of Special Character)
DES5 (Delivering High Quality Homes)
DES8 (Construction Management)
TAP1 (Access, Parking and Servicing)
CCF1 (Climate Change Mitigation)
CCF2 (Flood Risk)
NHE2 (Protecting and enhancing biodiversity)
NHE3 (Protecting trees, woodland areas and natural habitats)
INF3 (Electronic communication networks)

5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide

Vehicle and Cycle Parking

Guidance 2018

Householder Extensions and
Alterations

Other

Human Rights Act 1998

Community Infrastructure Levy
Regulations 2010

6.0 **Assessment**

6.1 The site is within the urban area. In this context, a residential redevelopment is acceptable in principle, subject to consideration of detailed matters

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Highway matters
- Impact on trees
- Amenity for future occupants
- Biodiversity
- Drainage and flooding
- Community Infrastructure levy
- Affordable Housing

Design appraisal

6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New

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development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.

- 6.4 DMP Policy DES2 which relates to development of residential garden land seeks to ensure that backland developments are of high-quality. The policy requires, amongst other things, that garden land developments should be designed to respect the scale, form and external materials of existing buildings in the locality and a height, mass and bulk to be in keeping with the existing street scene. DES2 requires that developments do not create an undue disruption in the street scene and that developments should ensure that a good standard of amenity is retained for existing and future occupants. The policy also encourages the retention of mature trees and hedges. Policy DES3 relates to Residential Areas of Special Character (RASC) and requires, amongst other things, buildings that are individually designed, and the design of the buildings and landscape to makes a positive contribution to the character of the area and promote local distinctiveness. The height, depth, elevations, scale and massing of development must respect the form of neighbouring buildings and the character of the RASC; existing tree cover, landscaping, green areas and vegetation to be retained or replaced, and where possible enhanced, using appropriate species; soft and hard landscaping to be sensitive to the plot, its setting and prevailing plot boundary characteristics; and the proposal must not involve inappropriate sub-division of existing curtilages to a size below that prevailing in the area.
- 6.5 In this instance the proposal would see the subdivision of No. 38 and the erection of a pair of semi- detached houses in the rear garden. Access would be gained towards the southern side of the existing dwelling at No. 38.
- 6.6 The proposal follows the application in 2018 for the erection of two detached dwellings on this plot. The application was refused and appeal dismissed. In dismissing the appeal the Inspector made the following points:

11. The development proposal would provide two substantial detached dwellings, one of which has an attached flat roofed garage, the other a detached garage to the front of the property. Together with the extent of the hardstanding to the frontage, I consider that the built form of the development proposal to be significant, taking up almost the entire width of the plot. Considering the proposal in terms of its depth, this is also significant, but not that unlike the neighbouring and recently completed properties at 36 Alma Road.

12. These dwellings are able to retain generous spacing between them and to each of their boundaries. The garden spaces are significantly larger and deeper. The proposal would create a large amount of development on this garden plot, with little regard for the generous spacing between buildings

which is highly characteristic of the area. Whilst being detached, the spacing between the proposed houses is restricted as a result of much of the gap being infilled at ground floor level by a garage. This differs to the clear gaps present in the permitted examples at 36 Alma Road, 5 Pilgrims Way and 8 Pilgrims Way. I consider this is not characteristic of the density and spaciousness of the area. I acknowledge that there have been changes over time to the density, however I do not find that this is sufficient reason alone for the development to be acceptable.

13. I have had regard to the extent of the mature vegetation to the rear boundary of the proposed development. I consider that the close proximity of the proposed development to these rear trees could result in undue pressure from pruning or felling. This is in particular due to the garden sizes, the proximity of the trees to the houses and the westerly facing rear gardens. This adds to my concerns about the spacing of the plots and the discord with the character of the RASC.

14. The appeal site represents an important area where there is substantial greenery through mature vegetation within the site and TPO trees to the frontage. This greenery makes a significant contribution to the area's character. There are glimpses through the site to the rear, and during summer months the leaf cover on trees will add to the importance of this sense of space and green – an identified characteristic of the RASC. Whilst the dwellings would be located to the rear of the existing dwelling and the TPO group, I consider they would be glimpsed through this gap, which would become closed. Thus, the sense of space will be reduced. The closeness of the two dwellings, and their close relationship to 38 Alma Road, would result in an almost continuous sense of built form and massing. Notwithstanding existing and proposed landscaping, I find that there would be harm to the character and appearance of the area and the RASC, through the loss of the lower density of dwellings combined with the loss of the generous spacing of plots throughout.

15. As considered alongside Policy Ho15 of the Reigate & Banstead Local Plan I am not persuaded that the proposal is able to demonstrate regard to the criteria of this policy (in particular i), ii), iii) and v)). I also find conflict with the other relevant policies, namely Ho13 (Maintenance of Character and Amenity), Ho14 (Development involving Back Garden Land) and Ho9 (Design and Layout) as a result of the massing of the proposal and its relationship to the host plot size and the character of the RASC and the loss of spacing in this location. The Framework expects that development should be resisted where poor design fails to take the opportunities available for improving the character and quality of an area and the way it functions, also taking into account any local design standards and style guides.

- 6.7 This proposal contrasts from that of the appeal as the current application proposes a pair of semi-detached houses. The resulting built form would be that of a single building that would have increased gaps to the side boundaries when compared to the previous application. The proposal would increase gaps to the north and south boundaries from 7.8m to the north boundary and 6m to the south in the refused scheme, to 10.5 m (+ 2.7m) to

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the north and 12.6m (+6.6m) to the south. Furthermore, both dwellings would have either a single, or one and a half storey element on the north or south side, further increasing the visual separation at first floor level. A detached garage was also previously proposed in the refused scheme in the north eastern corner of the site, this has been removed.

- 6.8 In the case of the dwelling towards the northern part of the site, the rear garden has been increased from between 13.8m and 15m to between 17.1m and 20.2m (+3.3m and 5.2m). Combined with the increase in gaps to the side boundaries, the removal of the detached garage, the site layout is considered to be more spacious and would accord with the pattern of development in the wider area and RASC.
- 6.9 The design of the dwellings would be traditional with hipped roofs and an eaves and ridge height that would decrease towards the south as the land decreases in height. The plans show the dwellings would be finished in brick and tile hanging with porch details to the front elevation. The dwellings would have an individual design however a cohesive appearance in terms of the style. The two dwellings would be similar in scale and appearance although with a variance in some features such as the catslide roof, porch design and layout; this variety is considered a positive design approach and akin to the variety of designs seen in the streetscene.
- 6.10 To the front of the site would be space for parking and turning as well as areas of landscaping. Were the application to be approved, a condition is recommended to secure the details of landscaping and planting.
- 6.11 The site is not within nor adjacent to a Conservation Area and is not considered to result in a harmful impact in this regard.
- 6.12 Overall, it is considered the proposed development would be acceptable in terms of its design and impact upon the character of the wider area, and complies with policies DES1, DES2 and DES3 of the Development Management Plan.

Neighbour Amenity

- 6.13 To the southern side of the site is 36A Alma Road, a detached property built relatively recently. There would be a separation distance of approximately 18.6m between the flank wall of the proposed development and the flank wall of No. 36A. Given this level of separation, the proposal is not considered to result in a harmful impact upon the amenities of this dwelling.
- 6.14 To the north of the site is 3 Beverley Heights. There would be a gap to the northern boundary of 10.5m between the flank wall of the garage and the shared side boundary. This distance would be increased to 13.2m at first floor level, at the closest point. There would be a distance of approximately 14m between the dwellings at the closest point. No. 3 sits further eastwards than the proposed new dwellings, creating a staggered relationship between the dwellings. In view of the level of separation and juxtaposition between the

dwellings, the proposal is not considered to result in a harmful impact upon the amenities of this dwelling.

- 6.15 During the course of the application, the proposed side facing dormer window has been removed. One first floor side facing window is proposed, this would serve a bathroom and would be conditioned to be of obscure glazing to avoid issues of overlooking and loss of privacy.
- 6.16 To the north east of the site is 1 Beverley Heights. This dwelling occupies a higher land level and the rear elevation looks obliquely towards the front of the application site. There would be a distance of approximately 38m between the rear elevation of No. 1 and the front elevation of the proposed dwellings at the closest point. This distance is considered sufficient to avoid a harmful impact upon the amenities of No. 1.
- 6.17 The donor property, 38 Alma Road, would have a separation distance of approximately 40.5m between the rear elevation of No. 38 and the front elevation of the proposed development. The rear garden of the donor property wraps around the rear garden of No. 40 Alma Road, resulting in a separation distance of approximately 24m between the front elevation of the proposed development and the rear boundary of No. 40 at the closest point and a distance of approximately 47m to the rear elevation. These distances are considered sufficient to avoid a harmful impact upon the amenities of these dwellings.
- 6.18 Objection has been raised from neighbouring properties regarding inconvenience during construction and noise and disturbance. Some inconvenience may occur during the construction of the proposal, however this is part and parcel of development and would not form a sustainable reason for refusal. Statutory nuisance legislation does however exist to control any significant harm that may occur and a construction method statement would be secured by planning condition were the application to be approved.
- 6.19 The proposed development may result in some additional noise and disturbance; however, the development would be in residential use and this would not be significant enough to warrant refusal of the application. Loss of a private view is not a material planning consideration.

Highway matters

- 6.20 The site is located in an area which is assessed as having a medium accessibility rating. In such areas, the Council's adopted parking standards require the provision of 2 spaces for each 5 bedroom house. The application proposes a total of 4 parking spaces, two each to the front of each dwelling. The attached garage meets the minimum internal space standards to be counted as a parking space so this would bring the total number of spaces to 5. The DMP requires a minimum of 4 parking spaces for a development of this size so the proposal meets the parking standards.

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- 6.21 The proposed development has been considered by the County Highways Authority who having assessed the application on safety, capacity and policy grounds, recommends conditions to secure the site is shall not be occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear, and the provision of fast charge sockets.
- 6.22 Notwithstanding that the Highway Authority makes no recommendations for a Construction Transport Management Plan, it is considered appropriate to impose such a condition for traffic safety and amenity justification.

Impact on trees

- 6.23 The site contains mature trees that make a valued contribution to the visual amenities of the area, some protected by way of Tree Preservation Order RE1418A. The application was submitted with a tree survey and arboricultural method statement. The Tree Officer was consulted upon the application and provided the following comments:

I have reviewed the proposed development and the potential impact on trees and vegetation both on and off site. There is a group of trees located close to the boundary of Alma Road which are subject formal protection by Tree Preservation Order Re1418A the group comprises of 1 ash, 1 oak (turkey) and 3 yew trees. None of these trees are lost to the proposed development. Although there is an incursion into the root protection area of the protected oak this incursion is within acceptable limits as set out with British Standard 5837:2012 trees in relation to design, demolition, and construction - Recommendations.

The application has been supported by detailed and thorough arboricultural information compiled by a known arboricultural Consultancy practice that undertakes work on a regular basis within the Borough. The information submitted has been compiled in accordance with the guidelines and advice contained within the above standard and tree have been assessed for their overall condition and suitability for retention within the proposed development adopting the criteria and methodology from section 4 and table 1 of the above standard.

The incursion into the root protection area of the protected oak as mentioned above is manageable subject to qualified supervision, protection measures and specialist construction techniques including the insertion of services.

The proposed development does result in some tree loss, 6 individuals are lost to facilitate the development all are of lower 'C' categories addition a tree loss is also proposed as a result of their overall condition and these works will be required whether development proceeds or not. Some facilitation pruning is required, however this pruning, if correctly implemented should not result in any loss of visual amenity or screening function currently afforded by the

affected trees, nor will these work result in any adverse effects on the future health and vigour of the affected trees.

The tree loss involved in this development within the exception of T5 ash and T8 horse chestnut are off internal landscape value only T5 and T8 are located close to the application site boundaries and are both 'u' category and their removal would be required, due to their overall condition and structure whether development proceeds or not.

There is opportunity to enhance and improve the existing landscape by replacement and addition al tree planting and this can be secured through and appropriate landscape condition.

The supplied arboricultural information contains an 'outline' arboricultural method statement. The Council will require a 'finalised arboricultural method statement and tree protection plan, particularly relating to the matters which involve specialist construction of above ground surfacing and the installation of service routing and drainage runs which are normally designed 'post ' decision, although the submitted TPP plan does indicate a service route.

The proposed development subject to 'key' stage supervision, ongoing qualified monitoring, specialist construction techniques and tree protection measures should be able to be achieved without lasting hard or significant disturbance to the rooting environments of retained trees and vegetation.

Due to the expected high levels of construction activity and processes I would strongly advise that a construction method statement condition should be imposed and should take fully into account the requirements of the 'finalised' AMS and TPP in fact the solution to the no dig surfacing and incursion into the RPA of the protected oak will need to be a collaborative effort between the civil engineer and arboricultural consultant.

- 6.24 The recommended conditions would be attached to a grant of permission. The Construction Transport Management Plan condition includes within it the requirement for details of loading and unloading of plant and materials and storage of plant and materials. These points would address the concerns raised in terms of the construction process to ensure that soil compaction, for example, does not occur from storage of materials under trees.

Amenity for future occupants

- 6.25 The NPPF provides that planning decisions should provide a high standard of amenity for future users. DMP Policy DES5 relates to the delivery of high quality homes and requires, inter alia, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units except where the Council accepts that an exception to this should be made in order to provide an innovative type of affordable housing that does not meet these standards. In addition, the policy also requires all new development to be arranged to

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ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.

- 6.26 Both dwellings would be generous in size and exceed the minimum space standards for 5 bedroom dwellings. The dwellings would occupy large plots with areas of outdoor amenity space and the proposal is therefore considered to comply with the requirements of policy DES5.

Biodiversity

- 6.27 Trees, hedges and woodland areas make a particularly valuable contribution to the character and visual amenity of the borough, both in the townscape and the landscape. They can also be valuable for biodiversity, providing important habitats for local wildlife and as part of wildlife corridors. The National Planning Policy Framework (NPPF) makes it clear (para 170) that "Planning policies and decisions should contribute to and enhance the natural and local environment by; minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures". Paragraph 174 requires the promotion of "the conservation, restoration and enhancement of priority habitats, ecological networks and the protection and recovery of priority species; and identify and pursue opportunities for securing measurable net gains for biodiversity".

- 6.28 During the course of the application an ecology report was requested and submitted. Consultation has been undertaken with Surrey Wildlife Trust since who have provided the following comments:

The developer should take action to ensure that development activities such as vegetation or site clearance are timed to avoid the bird nest season of early March to August inclusive.

If this is not possible and only small areas of dense vegetation are affected, the site could be inspected for active nests by an ecologist within 24 hours of any clearance works. If any active nests are found they should be left undisturbed with a buffer zone around them, until it can be confirmed by an ecologist that the nest is no longer in use.

Should the Local Planning Authority be minded to grant planning permission of the proposed development, Surrey Wildlife Trust recommends the Local Planning Authority require the development to only proceed in a precautionary manner that will avoid the killing or injuring of any individual reptiles that may be identified during development.

Paragraph 175 of the NPPF requires that "opportunities to incorporate biodiversity improvements in and around developments should be encouraged, especially where this can secure measurable net gains for biodiversity".

This development offers opportunities to restore or enhance biodiversity and such measures will assist the Local Planning Authority in meeting the above obligation and also help offset any localised harm to biodiversity caused by the development process.

Should the Local Planning Authority be minded to grant permission for the proposed development, the development should proceed only in strict accordance with the impact avoidance and mitigation measures specified in the ecology report.

Given the presence of ecological receptors on site (particularly the pond), there is a risk of causing ecological harm resulting from construction activities. Should the Local Authority be minded to grant permission for the proposal the applicant should be required to implement the development only in accordance with an appropriately detailed CEMP. This document will need to be submitted to and approved by the Local Authority in writing, prior to the commencement of the development.

- 6.29 Conditions are recommended to secure the development proceeds in accordance with the impact avoidance and mitigation measures specified in the submitted report and a second condition to secure a CEMP is submitted and approved prior to commencement of works.

Drainage and flooding

- 6.30 The site is not within Flood Zones 2 or 3 and is considered to have a satisfactory impact with regard to flooding. Sewage capacity would be dealt with under Building Regulations. The Council's Drainage Engineer has reviewed the submitted Flood Risk Assessment and finds no reason to refuse the application on drainage or flood risk grounds. To ensure that the site is satisfactorily drained with regard to Development Management Plan policy CCF2 and National Planning Policy Framework 2019, a condition is recommended to secure details of a scheme for the disposal of foul and surface water drainage from the site prior to commencement of development.

Sustainable Construction

- 6.31 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. No evidence has been submitted to demonstrate that that the proposed development can achieve either of the two requirements. However, in the event that planning permission is to be granted, a condition could be imposed to seek such information and its implementation prior to the first occupation of development. In this regard, there would be no conflict with DMP Policy CCF1.

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- 6.32 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services.

Community Infrastructure Levy

- 6.33 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable, although the exact amount would be determined and collected after the grant of planning permission.

Affordable housing

- 6.34 DMP Policy DES6 relates to the provision of affordable housing. This states that on all sites which provide 11 or more homes, 30% of the homes on the site should be affordable housing. This proposal would not therefore qualify for the provision of affordable housing.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan type	Reference	Version	Date received
Location plan	AAL-20-188-P01	A	18.08.2021
Streetscene	AAL-20-188-P06	A	18.08.2021
Elevation plan	AAL-20-188-P04	A	18.08.2021
Proposed plans	AAL-20-188-P05		04.03.2021
Proposed plans	AAL-20-188-P07		04.03.2021
Floor plans	AAL-20-188-P03	A	18.08.2021
Block Plan	AAL-20-188-P02		04.03.2021
Landscaping plan	862-L-01	A	04.03.2021
Landscaping plan	862-L-02		04.03.2021
Existing plan	270218		23.03.2021
Site layout plan	AAL-20-188-P08		06.05.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) measures to prevent the deposit of materials on the highway
 - (e) on-site turning for construction vehicles
 - (f) no HGV movements to or from the site to take place between the hours of 8.30 and 9.30 am and 3.00 and 4.00 pm (school start and finish times)
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019, Reigate and Banstead Core Strategy 2014 Policy CS17 and Reigate and Banstead Development Management Plan September 2019 policies TAP1 and DES8

5. No development shall commence until a Construction Environmental Management Statement, to include details of:
- a) Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
 - b) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
 - c) Means of communication and liaison with neighbouring residents and businesses.
 - d) Hours of work.
- Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

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6. No development shall commence including any groundworks preparation until a detailed, scaled 'finalised' Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) which shall be compiled in conjunction with the construction method statement and submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas (RPA) of trees shown to scale on the Tree Protection Plan (TPP), including the installation of hard surfacing within identified root protection areas, service and drainage routings, location of site welfare offices and storage of materials. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

7. No development shall commence on site until a scheme for the landscaping and replacement tree planting of the site including the retention of existing landscape features has been submitted and approved in writing by the local planning authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or use of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and

to comply with policies NHE3 and DES1 of the Reigate and Banstead Borough Local Plan 2019, British Standards including BS8545:2014 and British Standard 5837:2012

8. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:
- a) Evidence that there is no risk of contamination through the infiltration SuDs
 - b) Evidence that the proposed final solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development (Pre, Post and during), associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 37.8l/s.
 - c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element including details of any flow restrictions and maintenance / risk reducing features (silt traps, inspections chambers etc)
 - d) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational
 - e) Details of drainage management responsibilities and maintenance regimes for the drainage system
 - f) A plan showing exceedance flows (ie during rainfall greater than design events or during blockage) and how property on and offsite will be protected. This should include details of how surface water run-off entering the site from the bunded northern boundary will be intercepted.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

9. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme, or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

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The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

10. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

11. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

12. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3

13. Prior to the first occupation of the development full details (and plans where appropriate) of the waste management storage and collection points, (and pulling distances where applicable), throughout the development shall be submitted to and approved in writing by the Local Planning Authority.

All waste storage and collection points should be of an adequate size to accommodate the bins and containers required for the dwelling(s) which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

14. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter retained and maintained for their designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 and NHE9 of the Development Management Plan.

15. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
 - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

16. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
 - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in

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accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

17. The development shall be carried out in accordance with the impact avoidance, mitigation measures and enhancements set out within section 5 of the Preliminary Ecological Appraisal and Bat Survey Report dated August 2021.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

18. The first floor windows in the side elevations of the development hereby permitted shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at www.reigate-banstead.gov.uk. Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs

- Saturday and not at all on Sundays or any Public and/or Bank Holidays;
- (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
7. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837

Agenda Item 6

Planning Committee
1st September 2021

Agenda Item: 6
21/00546/F

8. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to:
<http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
9. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover to install dropped kerbs. www.surreycc.gov.uk/roads-and-transport/permits-and-licences/vehicle-crossovers-or-drop-ped-kerbs
10. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
11. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
12. The Applicant is advised that the CEMP should include details of how construction activities shall avoid ecological harm, particularly to the onsite pond.

REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies CS1, CS2, CS4, CS10, CS11, CS14, CS15, DES1, DES2, DES3, DES5, DES8, TAP1, CCF1, CCF2, NHE2, NHE3, INF3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



Appeal Decision

Site visit made on 5 December 2018

by Rebecca Thomas MRICS MRTPI

an Inspector appointed by the Secretary of State

Decision date: 20 March 2019

Appeal Ref: APP/L3625/W/18/3205366

38 Alma Road, Reigate, RH2 0DH

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Emmerton Developments against the decision of Reigate and Banstead Borough Council.
 - The application Ref 18/00469/F dated 27 February 2018, was refused by notice dated 24 April 2018.
 - The development proposed is the erection of two detached dwellings with associated parking and landscaping following the demolition of existing garage
-

Decision

1. The appeal is dismissed.

Application for costs

2. An application for costs was made by Emmerton Developments against Reigate and Banstead Borough Council. This application is the subject of a separate Decision.

Procedural Matters

3. Since the submission of the appeal, the National Planning Policy Framework from 2012 has been replaced, with the new version being published in July 2018 and updated in February 2019 (the Framework). Paragraph 212 of the Framework outlines that the policies contained within it are material considerations which should be taken into account in dealing with applications from the day of its publication.
4. The Council has confirmed that they withdraw their objection and second reason for refusal. Therefore, I have limited my assessment of the appeal proposals to the remaining reason for refusal.

Main Issue

5. The appellants submitted further arboricultural evidence with the appeal, following which the Council confirmed that its concerns had been overcome and that it would no longer contest the reason for refusal in respect of the effect on TPO trees. I see no reason to disagree. Accordingly, the main issue is the effect of the proposal on the character and appearance of the area which is within the Alma Road and Raglan Road Residential Area of Special Character (RASC).

Agenda Item 6

Appeal Decision APP/L3625/W/18/3205366

Reasons

6. The proposal is to erect two detached dwellings in the rear garden of 38 Alma Road. There is an existing garage and outbuildings which will be demolished to make use of the existing access point to provide a separate shared access for the two dwellings. To the road frontage is a group of trees subject to a Tree Protection Order (TPO).
7. The appeal site is a large rear garden characterised by a number of trees along the boundaries as well as within the site itself, these being a mixture of species and sizes. These characteristics are identified by the Borough's Local Distinctiveness Design Guide (the Design Guide), which describes the low density layout within heavily wooded areas.
8. Immediately adjacent to the appeal site, I saw the development at number 36 Alma Road, providing two dwellings. Nearby dwellings are a mixture of detached and semi-detached dwellings with a variety of designs, ages and materials. The buildings are set back from the street, and are well spaced from each other. To the appeal side of the street, there is no discernible building line and plots are large and spacious.
9. Travelling through the area, the character is one of a quiet residential road with space and dwellings set in large plots, often well screened by trees, hedges and high vegetation. This sense of space and greenery is identified by the Design Guide as being essential to the character of RASCs such as this.
10. To the rear of the appeal site, again properties vary, including a development of apartment blocks with associated garaging. I was able to glimpse the new development at 36 Alma Road from this side, however I noted the extensive vegetation on the boundary which obscures the appeal site. The Design Guide has identified that the spacing, mature landscaping and low density of properties in RASCs are important. The guide recognises that these have been subject to change through sub-division of plots, and the addition of apartment blocks has resulted in the loss of vegetation and mature trees and increased the density in some areas.
11. The development proposal would provide two substantial detached dwellings, one of which has an attached flat roofed garage, the other a detached garage to the front of the property. Together with the extent of the hardstanding to the frontage, I consider that the built form of the development proposal to be significant, taking up almost the entire width of the plot. Considering the proposal in terms of its depth, this is also significant, but not that unlike the neighbouring and recently completed properties at 36 Alma Road.
12. These dwellings are able to retain generous spacing between them and to each of their boundaries. The garden spaces are significantly larger and deeper. The proposal would create a large amount of development on this garden plot, with little regard for the generous spacing between buildings which is highly characteristic of the area. Whilst being detached, the spacing between the proposed houses is restricted as a result of much of the gap being infilled at ground floor level by a garage. This differs to the clear gaps present in the permitted examples at 36 Alma Road, 5 Pilgrims Way and 8 Pilgrims Way. I consider this is not characteristic of the density and spaciousness of the area. I acknowledge that there have been changes over time to the density, however I

- do not find that this is sufficient reason alone for the development to be acceptable.
13. I have had regard to the extent of the mature vegetation to the rear boundary of the proposed development. I consider that the close proximity of the proposed development to these rear trees could result in undue pressure from pruning or felling. This is in particular due to the garden sizes, the proximity of the trees to the houses and the westerly facing rear gardens. This adds to my concerns about the spacing of the plots and the discord with the character of the RASC.
 14. The appeal site represents an important area where there is substantial greenery through mature vegetation within the site and TPO trees to the frontage. This greenery makes a significant contribution to the area's character. There are glimpses through the site to the rear, and during summer months the leaf cover on trees will add to the importance of this sense of space and green – an identified characteristic of the RASC. Whilst the dwellings would be located to the rear of the existing dwelling and the TPO group, I consider they would be glimpsed through this gap, which would become closed. Thus, the sense of space will be reduced. The closeness of the two dwellings, and their close relationship to 38 Alma Road, would result in an almost continuous sense of built form and massing. Notwithstanding existing and proposed landscaping, I find that there would be harm to the character and appearance of the area and the RASC, through the loss of the lower density of dwellings combined with the loss of the generous spacing of plots throughout.
 15. As considered alongside Policy Ho15 of the Reigate & Banstead Local Plan I am not persuaded that the proposal is able to demonstrate regard to the criteria of this policy (in particular i), ii), iii) and v)). I also find conflict with the other relevant policies, namely Ho13 (Maintenance of Character and Amenity), Ho14 (Development involving Back Garden Land) and Ho9 (Design and Layout) as a result of the massing of the proposal and its relationship to the host plot size and the character of the RASC and the loss of spacing in this location. The Framework expects that development should be resisted where poor design fails to take the opportunities available for improving the character and quality of an area and the way it functions, also taking into account any local design standards and style guides.

Other Matters

16. I have had regard to the decisions provided by the appellants in the nearby area. Many of these do not show identical effects on the character and appearance and so have little bearing on my findings. The site adjacent at 36 Alma Road involves the demolition of an existing dwelling, which the appeal site does not. Other differences I have previously discussed above.
17. A more recent appeal decision at 5 Alders Road (APP/L3625/W/17/3178895) is similar in that the site was located in the garden of the host site. However, this appeal involved the construction of one dwelling and the Inspector found that there were significant benefits including the loss of an incongruous and unattractive development (annex building) and there was the ability to continue to provide the generous spacing between and around dwellings, subject to reserved matters. I agree with the Inspector's observations about

Agenda Item 6

Appeal Decision APP/L3625/W/18/3205366

the character and appearance of the RASC, however the detailed findings of that appeal are sufficiently different from the circumstances here that justifies me reaching a different conclusion.

18. The proposal would make an efficient use of land, and would contribute to the borough's housing stock and increase consumer choice. The proposal has had regard to the high level of design in the area and includes a sensitive landscaping scheme. However, these benefits do not outweigh the harm that I have found.

Other matters

19. I have had regard to the other matters raised, which include privacy, highways safety, drainage and effects on nearby watercourses and threats to other trees but these do not add to my reasons for dismissing the appeal.

Conclusion

20. For the reasons set out above, I find that the proposal conflicts with the development as a whole and the appeal should be dismissed.

Rebecca Thomas

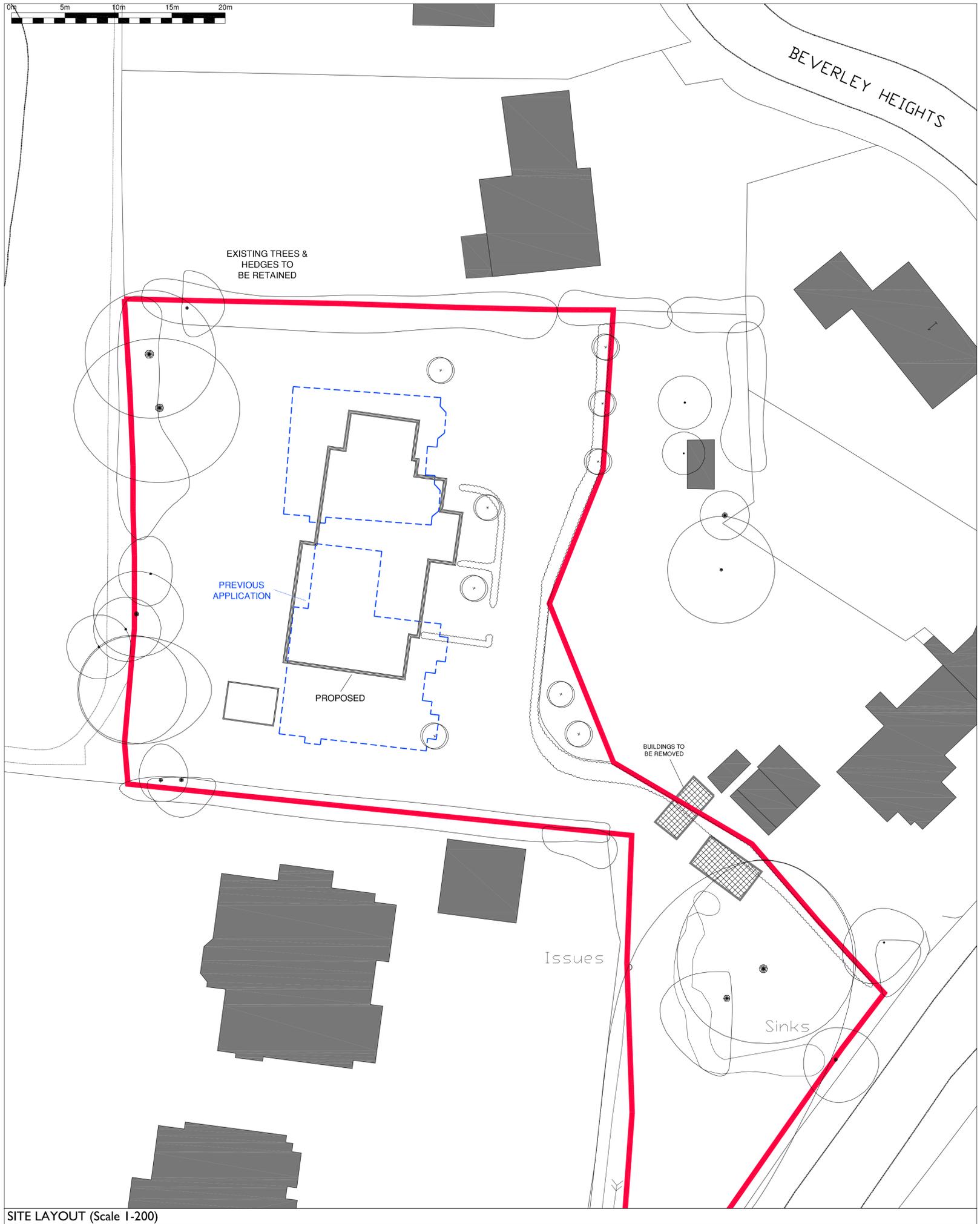
INSPECTOR

21/00546/F

38 Alma Road Reigate Surrey RH2 0DH



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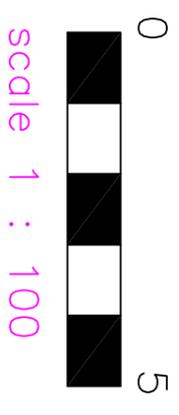
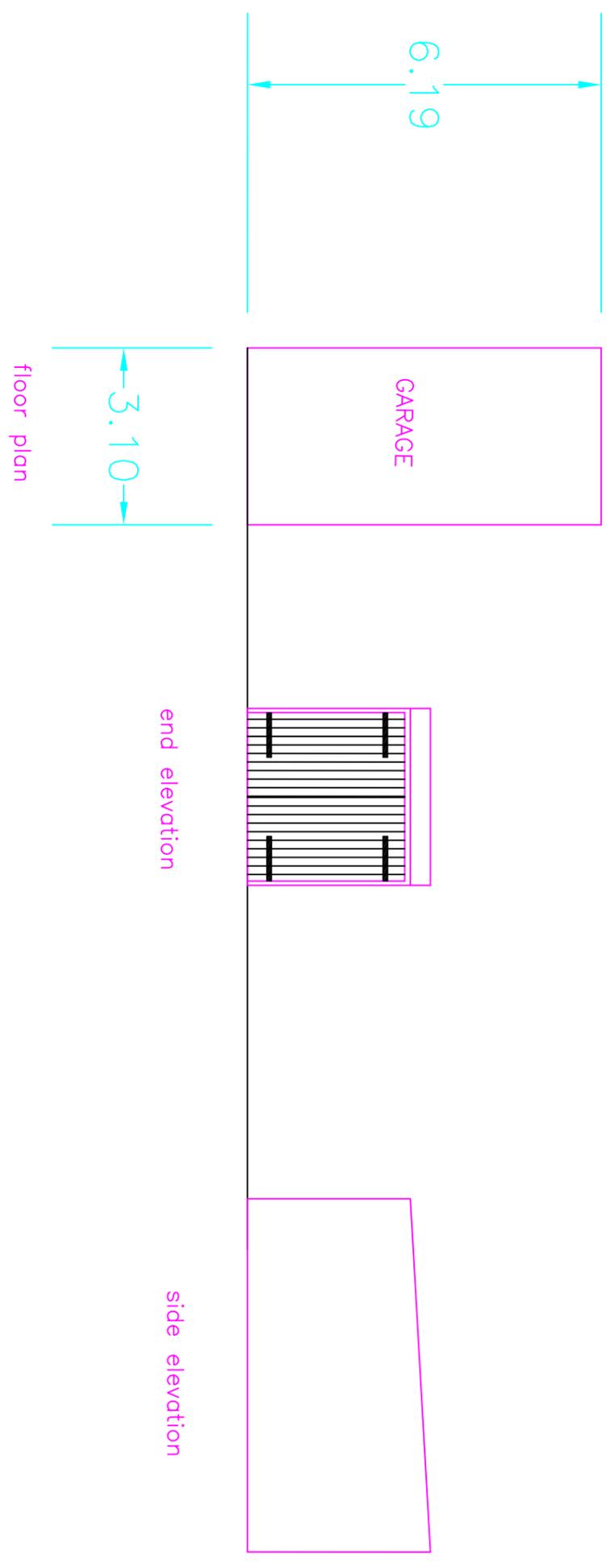
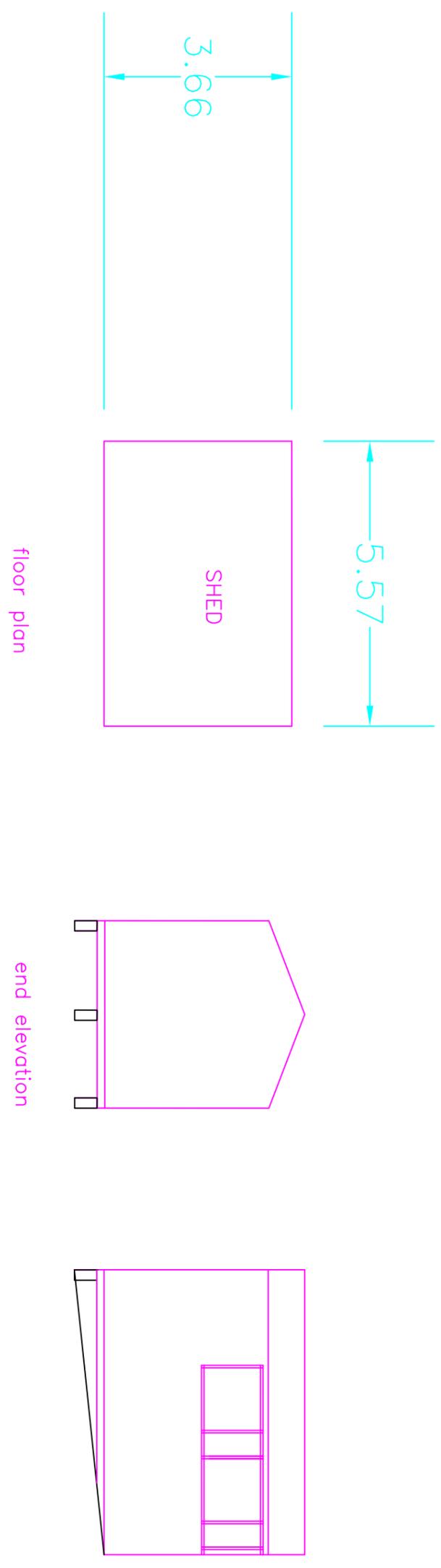


SITE LAYOUT (Scale 1-200)

REV.	DESCRIPTION	DRAWN	DATE
REVISIONS			
Issued for PROPOSALS			
SITE ADDRESS 38 ALMA ROAD REIGATE SURREY			
CLIENT EMMERTON DEVELOPMENTS			
SCALE	SHEET SIZE	DRAWN	DATE
A/S	A1	MC	JAN 2021
DRAWING TITLE COMPARISON PLAN			
DRAWING NUMBER AAL-20-188-P02		REVISION -	

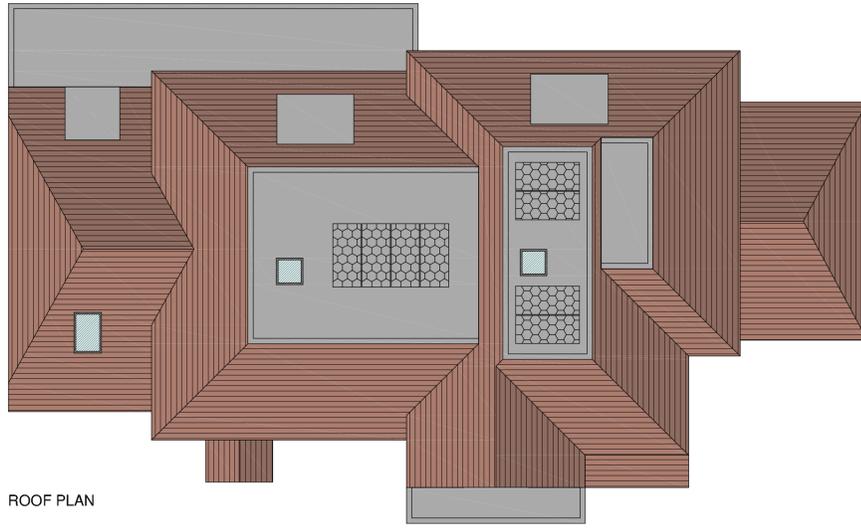
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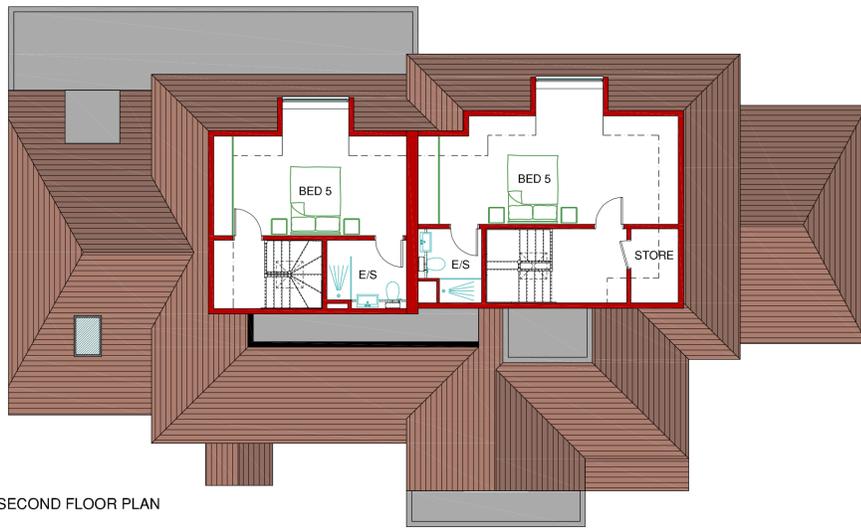


Plan of buildings to be demolished at 38 Alma road Reigate. Dwg
270218

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ROOF PLAN



SECOND FLOOR PLAN



FIRST FLOOR PLAN

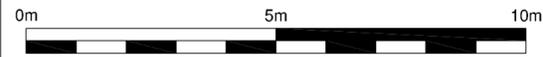


GROUND FLOOR PLAN

REV.	DESCRIPTION	DRAWN	DATE
A	Side-facing dormer removed from Plot 2	JK	18/09/2021

REVISIONS			
PROPOSALS			
SITE ADDRESS			
38 ALMA ROAD			
REIGATE			
SURREY			
CLIENT			
EMMERTON DEVELOPMENTS			
SCALE	SHEET SIZE	DRAWN	DATE
A/S	A1	MC	DEC 2020
DRAWING TITLE			
FLOOR PLANS & ROOF PLAN			
DRAWING NUMBER		REVISION	
AAL-20-188-P03		A	

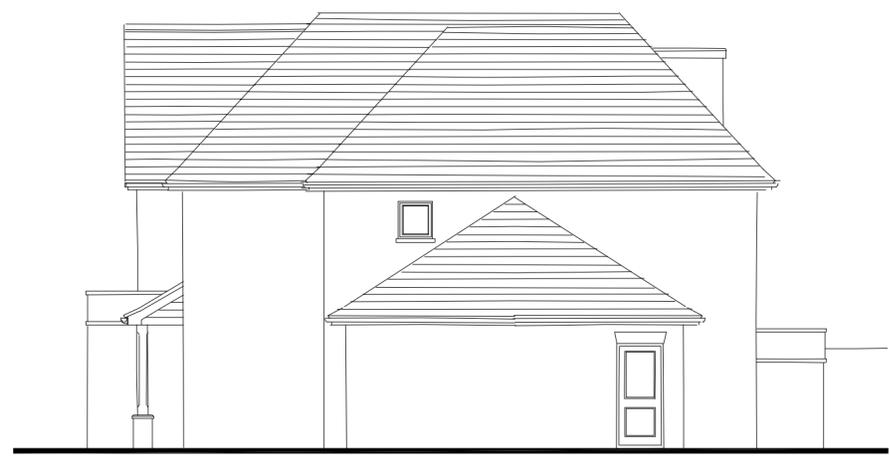
ALBURY ESTATE LOWER BARN WESTON FARM ALBURY SURREY GU5 9BZ T: 01483 205724 www.amasiaarchitects.com enquiries@amasiaarchitects.com	AMASIA ARCHITECTS LTD
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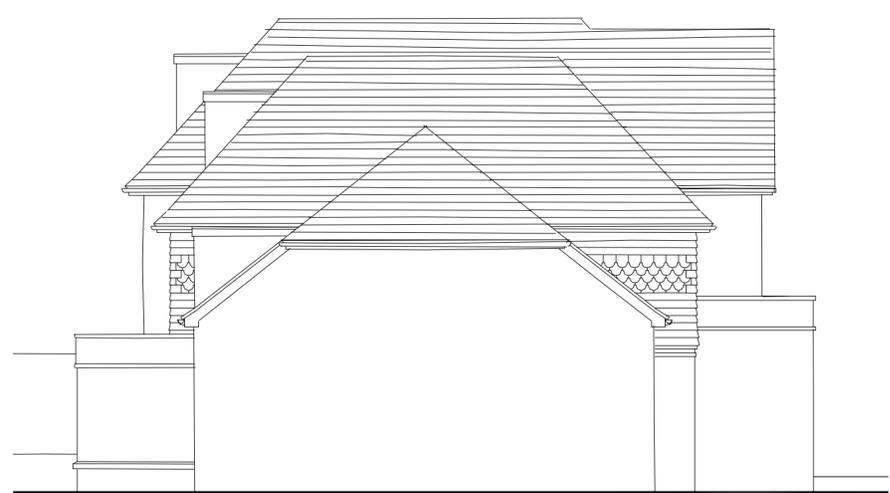
WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION

79

REV.	DESCRIPTION	DRAWN	DATE
A	Side-facing dormer removed from Plot 2	JK	18/08/2021

REVISIONS			
Issued for			
PROPOSALS			
SITE ADDRESS			
38 ALMA ROAD REIGATE SURREY			
CLIENT			
EMMERTON DEVELOPMENTS			
SCALE	SHEET SIZE	DRAWN	DATE
1-100	A2	MC	JAN 2021
DRAWING TITLE			
ELEVATIONS			
DRAWING NUMBER		REVISION	
AAL-20-188-P04		A	

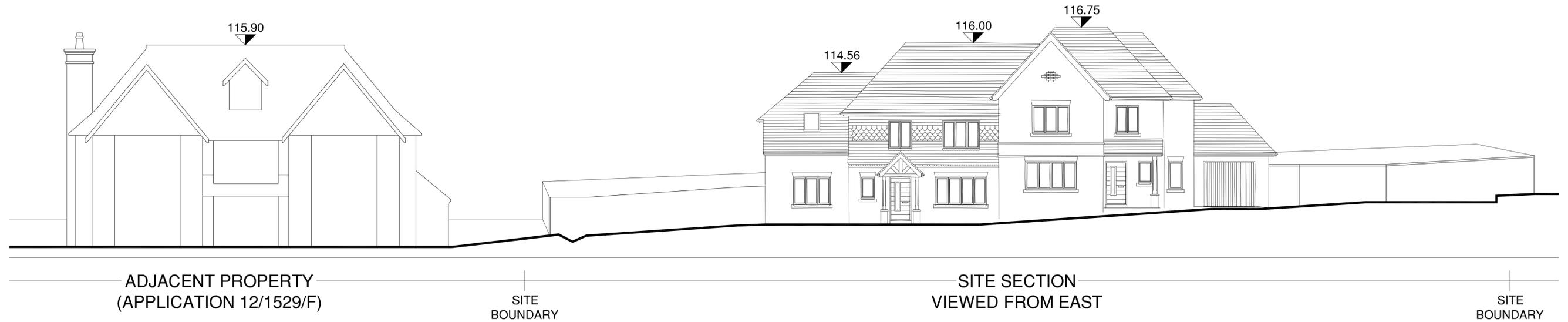
<p>AMASIA ARCHITECTS LTD</p>	<p>Agenda Item 6</p>
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Appendix Item 6



08

REV	DESCRIPTION	DRAWN	DATE
A	Side-facing dormer removed from Plot 2	JK	18/08/2021

REVISIONS

Issued for
PROPOSALS

SITE ADDRESS
38 ALMA ROAD
REIGATE
SURREY

CLIENT
EMMERTON
DEVELOPMENTS

SCALE | SHEET SIZE | DRAWN | DATE
1-100 | A1 | MC | JAN 2021

DRAWING TITLE
STREET SCENE

DRAWING NUMBER
AAL-20-188-P06

REVISION
A



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Agenda Item 7

Planning Committee
1st September 2021

Agenda Item: 7
21/01012/F

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:		PLANNING COMMITTEE	
	DATE:		01 September 2021	
	REPORT OF:		HEAD OF BUILDING & DEVELOPMENT SERVICES	
	AUTHOR:		Lesley Westphal	
	TELEPHONE:		01737 276769	
	EMAIL:		Lesley.Westphal@reigate-banstead.gov.uk	
AGENDA ITEM:	7	WARD:	Redhill East	
APPLICATION NUMBER:		21/01012/F	VALID:	18 June 2021
APPLICANT:	Vamos Invest Ltd		AGENT:	PRC Architecture & Planning Ltd
LOCATION:	42-44 HOLMETHORPE AVENUE, REDHILL, SURREY RH1 2NL			
DESCRIPTION:	Demolition of existing building and the redevelopment comprising 5 units within a single building and change of use from B2 general industrial use to light industrial (use class e), general industrial (B2) and storage and distribution (B8 use class) uses, together with ancillary offices, and associated parking and landscaping. as amended on 18.6.21 and 29.6.21.			
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.				

SUMMARY

This is a full application for the demolition of an existing industrial unit and erection of a new unit on the Holmethorpe Estate. The new building would occupy a larger proportion of the site than the existing scheme, comprising a single building to be divided into 5 separate units. Each unit would provide a loading bay, off street parking and, internally, ancillary offices and facilities together with space across two floors to be used for either B2 (General Industrial), B8 (Storage and distribution) or Class E (light industrial) uses. These uses would accord with the policy approach for this estate.

The proposed design would be of a more contemporary approach than the existing unit, and the general scale, mass and design of the building would be acceptable in the context of the sit and wider character of this estate, which currently shows a transition between the original brick built buildings and the many newer buildings using a more contemporary materials palette.

The site occupies a corner location and each unit would have vehicular access onto the adjacent access road which serves the existing unit as well as those units on the opposite side of the road. The level of parking proposed is acceptable. Some concern has been expressed by the Highways authority regarding the number of

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Planning Committee
1st September 2021

Agenda Item: 7
21/01012/F

loading bays, but they are content that this issue can be managed by the use of an appropriate condition.

Concern has been expressed by nearby occupants regarding the potential for additional traffic both during and after completion such as to cause difficulties and obstruction on the access road, interfering with access for nearby businesses. However, the scheme has been assessed by the County Highways Authority assessing data regarding potential traffic movements/parking requirements associated with the existing business and that of the proposed uses and they are satisfied that, subject to a range of relevant conditions, this scheme could operate without causing such interference and harmful consequences as to be unacceptable.

Subject to appropriate conditions regarding highway, ecology, materials and the use of the site, the scheme is considered to be acceptable.

RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: Note that using TRICS data the existing use should provide parking for 65 vehicles but only provides 18, with a shortfall of 47 spaces. The proposed use should provide 23 spaces but will provide 28, which is acceptable.

The scheme should provide 15 loading bays whilst only 5 are provided. It is therefore proposed that a Deliveries and Servicing Management Plan be submitted to demonstrate how the site would accommodate this shortfall,

The scheme has been assessed on the basis of safety, capacity and policy grounds and it is recommended that conditions relating to the following matters are attached to any approval:

- Provision of on site parking and turning prior to occupation
- Submission of cycle shelter details
- Submission of a Construction Transport Management Plan
- Submission of a Deliveries and Servicing Management Plan
- Submission of a Travel Statement
- Provision of at least 3 parking spaces with fast charge sockets

Informatives are recommended regarding the need to permission from the Highways Authority for any works to the highway, need to keep the highway clean and that the costs of any repairs to damaged highways resulting from the development will be passed on to the developer.

Surrey County Council Local Lead Flood Authority (LLFA): Are satisfied that the drainage scheme meets the requirements of the relevant legislation and no objections are raised subject to two conditions requiring full details of the design of a surface water drainage scheme and a verification report confirming these works prior to first occupation of the site. (Conditions 13 and 14).

Surrey Wildlife Trust: No objections providing recommendations made in the submitted ecological appraisal regarding the clearance of vegetation are followed (condition 12).

Thames Water: No objections relating to waste water and foul water disposal.

Details should be provided regarding the discharge of ground water into the public sewer (NOTE this is addressed by conditions 13 and 14).

UK Power Networks: Advise that the scheme lies in close proximity to a substation with advice regarding the company's notification and other requirements for new works.

Representations:

Letters were sent to neighbouring properties on 21 June 2021, a site notice was posted and the scheme was advertised in the local press on 1st July 2021.

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1st September 2021

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21/01012/F

Responses have been received from two nearby businesses raising the following issues:

Issue	Response
Highways safety issues	See paragraph 6.18
Increased traffic congestion	See paragraph 6.11 - 16
Potential obstruction of traffic accessing neighbouring sites/businesses	See paragraph 6.11-16
Noise and disturbance	See paragraph 6.22
Inconvenience during construction	See paragraph 6.17 & 6.22

1.0 Site and Character Appraisal

- 1.1 The site comprises a rectangular plot, some 0.47 ha in size, sitting on a corner plot in Holmethorpe Avenue on the Holmethorpe Industrial Estate. It comprises a single building with a saw tooth roof line in use as a car body shop. The site provides parking at the front of the unit and a yard at the rear. Vehicular access to the site is off Holmethorpe Avenue on the northern boundary which is a private road shared with the industrial units at 34,36 and 38 Holmethorpe Avenue.
- 1.2 The surrounding area comprises a range of commercial units in a variety of uses including commercial offices, warehouse and trade counter units. Immediately to the north of the site lies a car wash, stores and manufacturing unit with trade counter units and warehousing and offices to the south. Further offices, vehicle repair and builder's merchant are sited to the west. The buildings on this estate are of a variety of designs and sizes.
- 1.3 Immediately to the north of this site lie a series of commercial units with a parking area in front of the units. The site is heavily used by traffic with much parking evident on forecourts and kerbsides, in the absence of double yellow lines, which control much of the estate around this site.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage:
None – there was no pre-application enquiry
- 2.2 Improvements secured during the course of the application:
None the scheme is acceptable as submitted
- 2.3 Further improvements to be secured through conditions or legal agreement:
A variety of improvements to be secured through conditions relating to appearance and use of the site.

3.0 Relevant Planning and Enforcement History

- | | | | |
|-----|------------|---|-------------------------|
| 3.1 | 12/00295/F | Retrospective application for new fencing | Granted
20.June 2012 |
|-----|------------|---|-------------------------|

4.0 Proposal and Design Approach

- 4.1 This is a full application seeking permission for Demolition of existing buildings and the redevelopment comprising 5 units within a single building and change of use from B2 general industrial use to light industrial (use class E), general industrial (B2 Use Class) and storage and distribution (B8 use class) uses, together with ancillary offices, and associated parking and landscaping.
- 4.2 The proposed scheme would comprise a two-storey building aligned along the southern boundary in an east/west direction across the site. It would provide pedestrian access only to the rear of the site and each side of the building but with most space at the front of the building where the parking and servicing facilities are proposed, accessed off Holmethorpe Avenue.
- 4.3 The building would be divided into 5 units ranging in size between 493 sq m's – 675 sq m's in scale. Each unit would have a single lorry loading bay and between 4 and 7 car parking spaces (including at least one space for the disabled for each unit). Each building is shown to have offices arranged adjacent to a reception area and toilet facilities on the ground floor and a tea point on the first floor within an otherwise open first floor.
- 4.4 A small area of soft planting is shown on the corner of Holmethorpe Avenue Around the parking bays of the first unit and between the highway and the building on that part of Holmethorpe Avenue to the west of the site.
- 4.5 The site has been laid out so that the front of the building is clearly visible from the access road and parking areas are directly outside each of the unit entrances. First floor windows allow natural surveillance of the site access and parking/service areas.
- 4.6 The scale of the building has been designed to fit sympathetically into the scale of its surroundings with a monopitch roof of between 8.25m and 11.6m high. Surrounding buildings range between c.5 – 9.1m in height and upto 11.5m's for the data centre further south at St Anne's Boulevard. Surrounding roof forms are varied in design.
- 4.7 The proposed building would comprise a single building with a mono pitch roof and continuous roofline and a projection of the office element on the northern and western elevations address a prominent elevation at the north-western boundary (junction of Holmethorpe Avenue and the access road – also Holmethorpe Avenue) and would use a palette of materials including, profiled metal cladding, timber cladding, composite cladding, aluminium

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windows and microrib composite cladding in silver/grey shades. In addition to these neutral shades terracotta panels are proposed near each unit main entrance with a colour and texture similar to the brick walls found in the industrial buildings of the surrounding area. The choice of colour avoids primary colours which would prematurely date the development and affect its long-term aesthetic appeal.

4.8 Further details of the development are as follows:

Site area	0.47ha
Existing use	General Industrial
Proposed use	Light Industrial/General Industrial /Storage and Distribution
Existing parking spaces	18
Proposed parking spaces	28
Parking standard	23 required

5.0 Policy Context

5.1 Designation

Urban Area
Holmethorpe Industrial Estate
1:1000 year event surface water flooding in the access road

5.2 Reigate & Banstead Core Strategy

CS1 Presumption in favour of sustainable development
CS5 Valued people and economic development
CS8 Area 2 (Redhill)
CS10 Sustainable development
CS11 Sustainable construction
CS17 Travel options and accessibility

5.3 Reigate and Banstead Development Management Plan 2019

EMP1 Principal Employment Areas
EMP4 Safeguarding employment land and premises
EMP5 Local skills and training opportunities
DES1 Design of new development
DES8 Construction Management
TAP1 Access, parking and servicing
CCF1 Climate change mitigation
NHE2 Protecting and enhancing biodiversity and areas of geological
Importance

5.4 Other Material Considerations:

Supplementary Planning Guidance

A Parking Strategy for Surrey
Parking Standards for Development

Other:

Human Rights Act 1998

6.0 Principal Issues

6.1 The main issues to consider are:

- Principal of Development
- Design appraisal
- Access and parking
- Neighbour Amenity
- Other Issues

Principal of Development

6.2 Core Strategy Policy CS1 supports sustainable development whilst policies EMP1 and EMP4 of the Reigate and Banstead Development Management Plan 2019 support new employment related development on the Holmethorpe Industrial Estate. Policy EMP1 Specifically advising that *“planning permission will be granted for change of use to offices, industrial and storage and distribution and for the development of new, upgraded or extended floorspace within these uses”*.

6.3 In this instance the scheme proposes a change of use of an existing general industrial building into 5 conjoined units which would offer either B2 General Industrial use, B8 Storage and distribution use or a light industrial use which falls within Use Class E (g) (iii).

6.4 Given the compliance of the proposed use with this policy and the fact that the site lies within a sustainable location, approximately 2 miles north of Redhill town Centre with links to the M23 and M25 and the railway station in Redhill the principle of this development is considered acceptable. It would form a sustainable development subject to compliance with the policies and issues considered below.

Design Appraisal

6.5 Policy DES1 of the Reigate and Banstead Development Management Plan 2019 expects all new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. It should reinforce any local distinctiveness and have due regard for instance to the layout, density plot sizes, massing, height and roofscapes of the surrounding area.

6.6 The industrial estate features a wide range of building designs and materials – with many of the older buildings being brick whilst the more modern

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buildings use a variety of colour cladding. The roofscape of the surrounding area is very varied. The existing building is of its time and whilst not unattractive is also not of any particular architectural merit – certainly there is no objection to its removal.

- 6.7 Considered against the existing character of this estate it is considered that the design of the proposed building would be acceptable: of a relatively unfussy design and using materials that are sympathetic to the local area. The general scale, plot coverage and height is also considered to fit comfortably within its surroundings. The siting of the loading/parking areas would be acceptable and a small area of green landscaped space is proposed on the corner of the access road with Holmethorpe Road: which would be a welcome introduction into the area: albeit of very modest proportions.
- 6.8 Overall, it is considered that the scheme would, in design terms, comply with the relevant development plan policies.

Access and Parking

- 6.9 Policy TAP1 of the Reigate and Banstead Development Management Plan 2019 requires that new development provides safe and convenient access for all road users taking account of cumulative impacts to ensure that it would not unnecessarily impede the free flow of traffic on the public highway or compromise pedestrians or any other transport modes. It shouldn't materially exacerbate traffic congestion and should provide adequate access in particular with regard to circulation, manoeuvring, turning space, visibility splays and provision for loading/unloading for an appropriate range of vehicles.
- 6.10 It is clear when visiting this site and its immediate surroundings that the highway is busy with on street parking both on and off double yellow lines, as well as on forecourts and in designated parking areas around the estate. At the time of the officers visit to the site cars/vans were parked on the highway immediately in front of the site (no double yellow lines in this part of Holmethorpe Avenue) and the presence of a refuse truck was sufficient to block the access road for a short while.
- 6.11 Two local businesses have raised the issue of the potential for these units and associated traffic to block the access road to the other businesses nearby. Attention has been drawn to the fact drawn to the fact that the applications do not own the whole of the access road: that being shared with a nearby business. The correct ownership certificate has been served acknowledging this fact.
- 6.12 The County Highways authority have calculated the required parking spaces based upon the current use/floorspace and the proposed uses/floorspace and conclude that the current uses actually requires 47 more spaces than are available. This may account for the level of on street parking immediately in front of the unit, although it is not of course possible to know from a site visit

who the cars parked by this unit belong to: in the absence of double yellow lines this may simply be a convenient places to park for anyone working nearby. By contrast using the proposed floorspace it has been calculated that the proposed units would over provide the required number of parking spaces by 5 spaces but underprovide the number of required loading bays by 10 bays.

- 6.13 Each of the 5 units is allocated its own loading bay and parking spaces in front of the unit with direct access from the access road. It may be possible therefore to control the way in which the loading bays are used such that the shortfall may not contribute to additional congestion as may be expected by the scale of the shortfall. Accordingly the County highways officer has requested that a condition be attached to secure a Delivery and Servicing Plan from the applicants (see Condition 6).
- 6.14 On the basis therefore of a reduced parking need compared to the existing, more parking than is required and the submission of the Delivery/Servicing Plan it is considered that the scheme would not have an unacceptable impact upon the adjacent access/highway.
- 6.15 The other owner of the access road has submitted a copy of the legal agreement relating to the use of this shared access which indicates that just under half of the access is also owned by a nearby business which the dividing line running through the length of the access road. The submitted Deed acknowledges that both owners of the land have the right to use the access road, notwithstanding that it falls within the other's title and that the access road shall at all times be kept unobstructed. Likewise the cost of repair and maintenance appears to be shared between both parties.
- 6.16 On this basis whilst the concerns of the adjacent owners are understood, it is considered that subject to appropriate conditions and the recourse that exists between the owners of the access road, that it will be possible to carry out this scheme with minimal impact upon the day to day usage of the access.
- 6.17 Objections have been raised about disruption during the construction phase and that may well occur, but it is not a material planning reason that could justify a refusal of new development. It is hoped in any event that the conditions proposed will minimise such disruption, including a requirement for a Construction Transport Management Plan at Condition 5.
- 6.18 Overall the scheme has been assessed by the County Highways Authority on the basis of safety, capacity and policy grounds and the scheme has been found to be acceptable subject to the recommended conditions. Overall therefore it is considered that the proposed scheme would comply with the relevant development plan policies.

Neighbours Amenities:

- 6.19 Policy DES1 of the Reigate and Banstead Development Management Plan 2019 requires new development to provide an appropriate environment for

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future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings including by way of overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.

- 6.20 In this instance the new building would lie closer to the eastern most boundary and the commercial unit sited adjacent but would not adversely affect the use of that site by any of the issues identified above it lying adjacent to the flank wall of that unit.
- 6.21 Likewise, none of the other buildings surrounding this site would be affected by the proposed new building in terms of the issues identified above. The greatest impact would arise during construction and as a result of a changed parking and access layout. However as discussed above it is considered that the changed access/parking arrangements whilst different could be managed in a way that would not adversely affect nearby businesses.
- 6.22 As addressed in the section above the issue of impact upon nearby businesses during the construction phase has been raised. It is anticipated that the use of a CTMP condition would be sufficient to minimise such impacts as far as is reasonably possible. There is potential for noise, disturbance and some disruption during construction, but such matters are regulated by other legislation and would not be a matter to warrant a refusal of permission.
- 6.23 Overall, it is not considered that the scale, layout or position of the building on the site, nor activities during the construction phase would be such as to adversely affect the amenities of any surrounding businesses such as to justify a refusal of permission.

Other Matters:

Ecology

- 6.24 Policy NHE2 of the Reigate and Banstead Development Management Plan 2019 requires that wherever possible a net bio-diversity gain should be achieved. The majority of this site is hardstanding with little of ecological interest and any plant life is limited to scrub and self-seeded plants. The distance of the site from any statutory sites or non-statutory sites and the intervening development make it unlikely that the scheme would have any impact upon them. Therefore, providing the removal of scrub vegetation in the south of the site which may provide suitable nesting site is removed outside the nesting period (March to July) the scheme would be acceptable. The site and scheme has limited opportunity for bio diversity and ecological enhancements, but there may be some opportunity in the new landscaped area to the front of the site to provide compensatory planting with some potential nesting area for birds as an enhancement.

Drainage/Flooding

- 6.25 Policy CCF2 of the Reigate and Banstead Development Management Plan 2019 requires that new development must avoid areas at risk of flooding, not increase the existing and future risk of flooding outside the site and should reduce surface water run-off rates using Sustainable Drainage systems.
- 6.26 The site lies within Flood Zone 1 (i.e that land least likely to flood) and is not identified itself as suffering from surface water flooding, albeit part of the adjacent access road is identified as experiencing a potential 1:1000 year flood event. The County Drainage Team have assessed this application and are satisfied that, subject to two conditions (13 & 14), the scheme would be acceptable.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby approved shall be carried out in accordance with the following approved plans:

11361-PI-001
11361-P1-002
11361-P1-003
11361-P1-004
11361-P1-005
11361-PL-010
11361-PL-0020
11361-P1-0021
11361-P1-0022
Y-F-4927-15-02,
1045-C-0100.P03

Reason: To define the permission and ensure the development is carried out in accordance with the approved plans and in accordance with the National Planning Practice Guidance.

3. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plan numbered PL 002 for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the car parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework

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2021 and Policy TAP1 Parking, access , and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

4. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority for a minimum of six bicycles to be stored in a sheltered and accessible location. Thereafter the bicycle parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019.

5. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (f) HGV deliveries and hours of operation
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 - (j) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

6. The development hereby approved shall not be first occupied unless and until a Deliveries and Servicing Management Plan specifying arrangements for deliveries to and removals from the site, to include details of:
- (a) of how deliveries and collections would be arranged to and from the site along with
 - (b) The design of delivery areas within the development site
 - (c) The dimensions and layout of lorry parking area(s) and turning space(s) have been submitted to and approved in writing by the Local Planning Authority.

Thereafter the approved details shall be implemented

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019 .

7. Notwithstanding the submitted travel plan prior to the occupation of the development a Travel Statement shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide", and in general accordance with the 'submitted travel plan. And then the approved Travel Statement shall be implemented upon first occupation and for each and every subsequent occupation of the development, thereafter maintain and develop the Travel Statement to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

8. The development hereby approved shall not be occupied unless and until a minimum of three of the proposed parking spaces are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) and another minimum of three of the parking spaces are provided with an electrical supply to retrospectively fit an electric vehicle charging point in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

9. The development hereby approved shall only be used for purposes falling within Classes B2, B8 and E (g) (iii) of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).

Reason: To ensure appropriate uses in this Principal Employment Area and policy compliant levels of parking, in compliance with Policies EMP1 and TAP1 of the Reigate and Banstead Development Management Plan 2019.

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10. No development shall take place above slab level unless details have been submitted to and approved in writing by the Local Planning Authority of all proposed external materials (including hard landscaping). The scheme shall be carried out in accordance with the approved details.

Reason: To ensure a satisfactory appearance upon completion in accordance with the provisions of Policy DES1 of the Reigate and Banstead Development Management Plan 2019 and the National Planning Policy Framework.

11. Prior to the first use of the development hereby approved, details shall be submitted to and be approved in writing by the Local Planning Authority of the proposed soft landscaping scheme. The scheme shall be carried out in accordance with the approved details.

Any planting that dies, become diseased, damaged or is removed within 5 years of being planted shall be replaced with similar species.

Reason: To ensure a satisfactory appearance upon completion in accordance with the provisions of Policy DES1 of the Reigate and Banstead Development Management Plan 2019 and the National Planning Policy Framework.

12. Vegetation in the south of the site which may provide suitable bird nesting sites shall be removed outside the nesting period (March to July).

Reason: To protect nesting birds in accordance with the aims and objectives of Policy NHE2 of the Reigate and Banstead Development Management Plan 2019 and the National Planning Policy Framework.

13. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SUDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

- a. A) Evidence that the proposed final solution will effectively manage the 1:30, 1:100 (+20% allowance for climate change) storm, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. The associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 2.2l/s.
- b. Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element including details of any flow restrictions and maintenance/risk reduction features (silt traps, inspection chambers, etc).
- c. A plan showing exceedance flows (ie during rainfall greater than design events or during blockage) n how property on and off site will be protected from increased flood risk.

- d. Details of drainage management responsibilities and maintenance regimes for the drainage system
- e. Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational

Reason: To ensure the design meets the National Non Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off the site.

14. Prior to first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved in writing by the Local Planning authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the Drainage System is designed to the National Non-Statutory Technical Standards for SuDS.

INFORMATIVES

- 1 The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover or to install dropped kerbs. Please see www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs
- 2 The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
- 3 Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.

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- 4 Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
- 5 The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.

REASON FOR PERMISSION

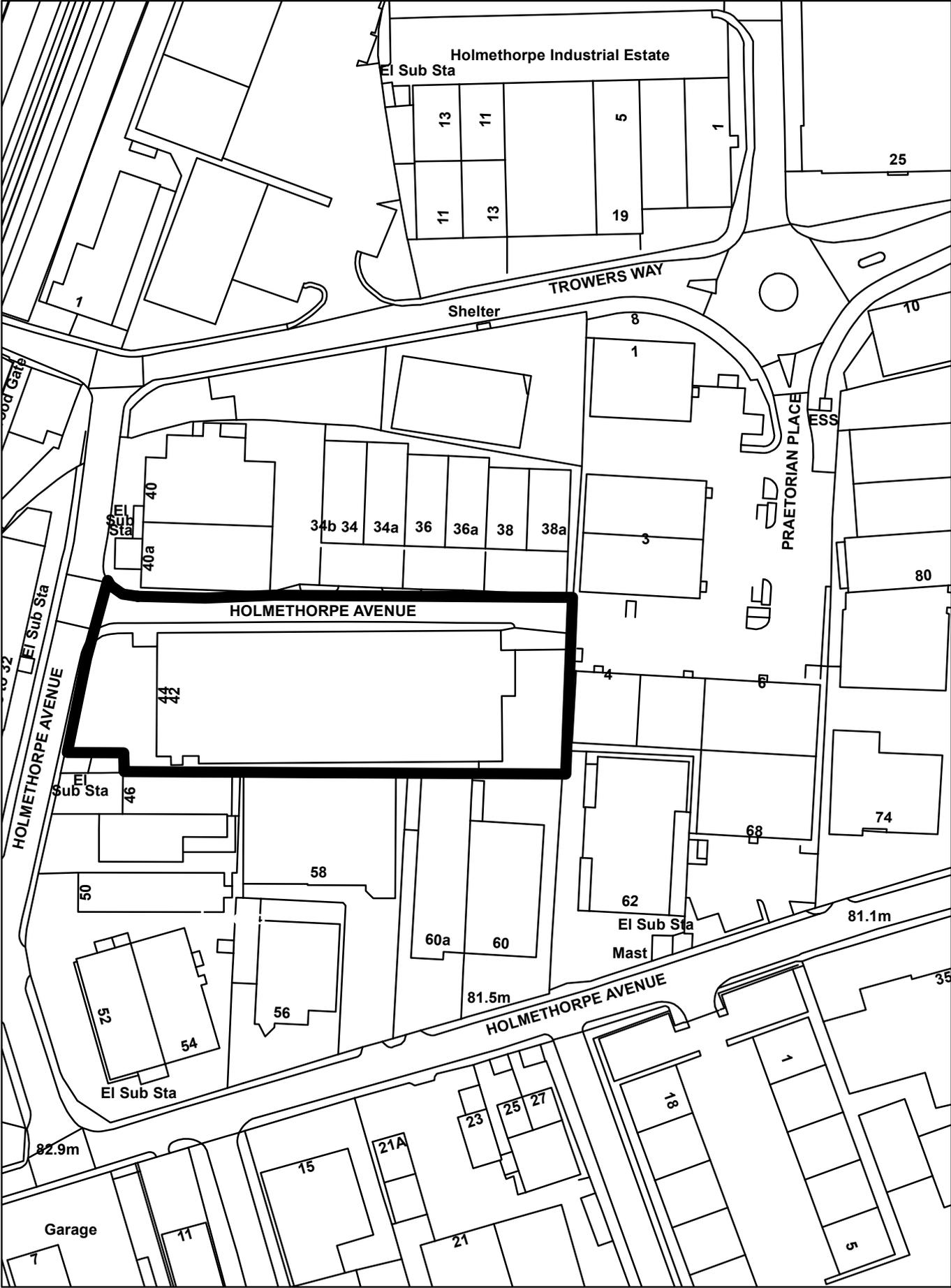
The development hereby permitted has been assessed against development plan policies CS1, CS5, CS8, CS10, CS11, CS17, EMP1, EMP4, EMP5, DES1, DES8, TAP1, CCF1, NHE2 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

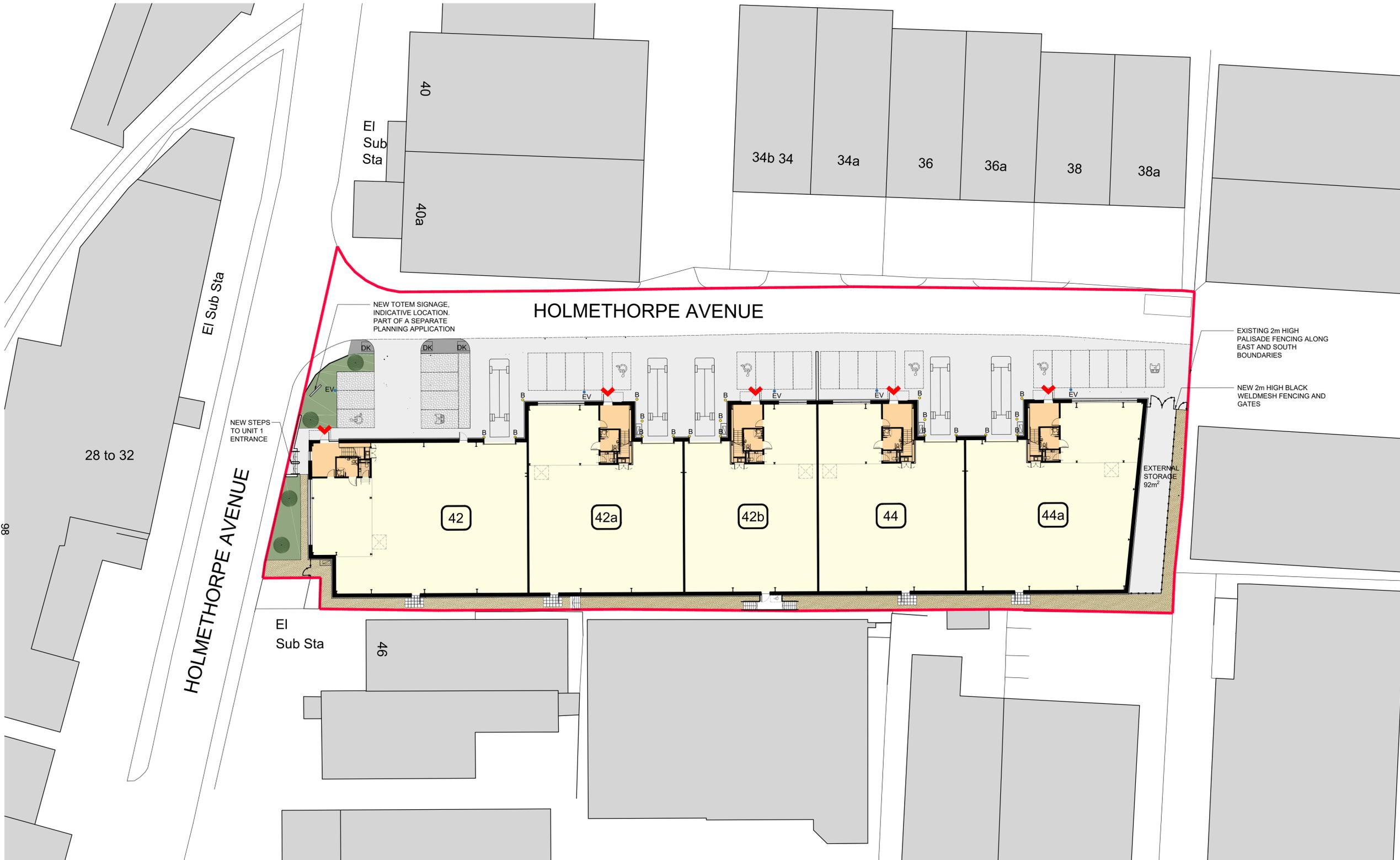
Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

21/01012/F

Auto Body Language Ltd 42-44 Holmethorpe Avenue RH12NL





- KEY**
- APPLICATION SITE
 - 2m BLACK WELDMESH FENCE WITH MATCHING GATES
 - EV ● ELECTRIC VEHICLE CHARGING POINTS
 - B ● BOLLARD
 - DK TACTILE PAVING DROP KERB TO PEDESTRIAN CROSSING POINTS
 - ACCESSIBLE PARKING BAY
 - CAR SHARING BAY
 - WELL CONSOLIDATED GRAVEL
 - CONCRETE SERVICE YARD
 - TARMAC FOOTPATH
 - CONCRETE BLOCK PAVING (POROUS)
 - PROPOSED SOFT LANDSCAPING
 - PROPOSED NEW TREES

Client:
VAMOS INVEST LTD

Project:
HOLMETHORPE AVENUE, REDHILL

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246
info@prc-group.com
www.prc-group.com



Drawing Title:
SITE LAYOUT PLAN

0 5 10 15 20 25m
Scale 1:250

	GEA			Car spaces
	Ground (m²)	First (m²)	Total (m²)	
1.	553.24	122.16	675.40	7
2.	468.69	111.95	580.64	5
3.	400.88	92.43	493.31	4
4.	442.22	103.12	545.34	5
5.	514.21	123.56	637.77	7
T	2,379.24	553.22	2,932.46	28

	GIA			
	Ground (ft²)	First (ft²)	Total (ft²)	Total (m²)
1.	5,590	1,155	6,745	626.63
2.	4,785	1,060	5,845	543.02
3.	4,090	880	4,970	461.73
4.	4,520	990	5,510	511.89
5.	5,180	1,165	6,345	589.47
T	24,165	5,250	29,415	2,732.74

SITE AREA (red line) 0.47Ha / 1.16A

Architecture
Planning
Master Planning
Urban Design
Interiors
Landscape

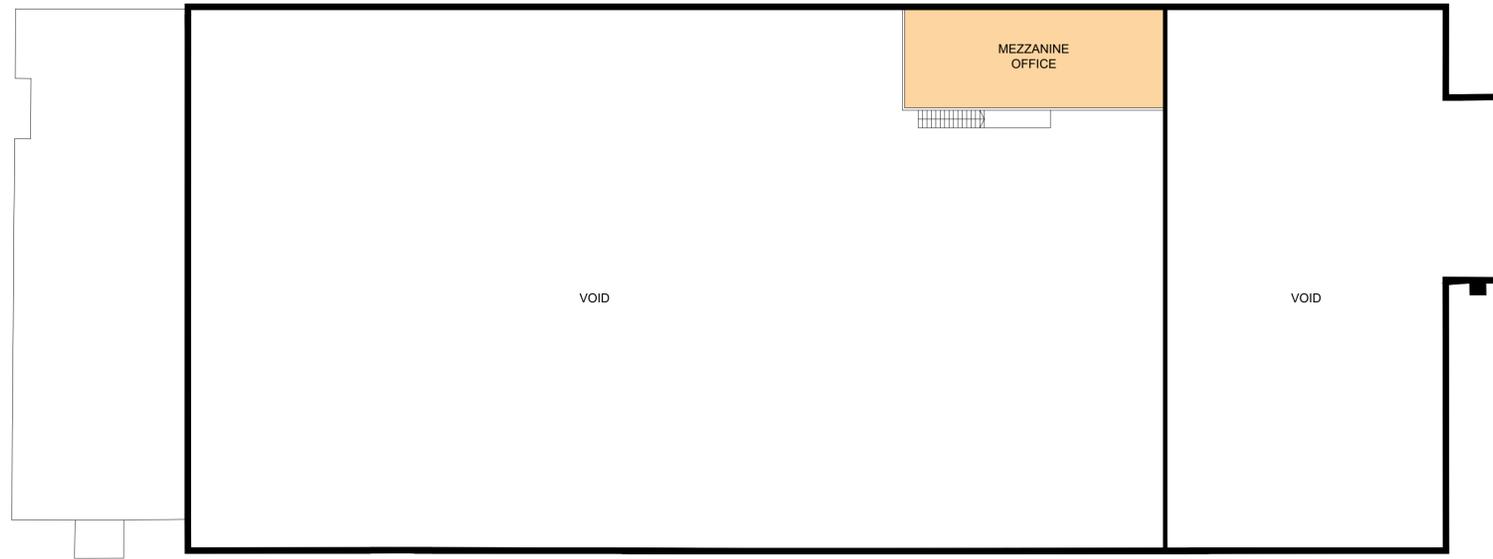
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Checked by: JR
Date: APR 21

Job No: 11361
Stage: PL 002
Drawing No: Rev:

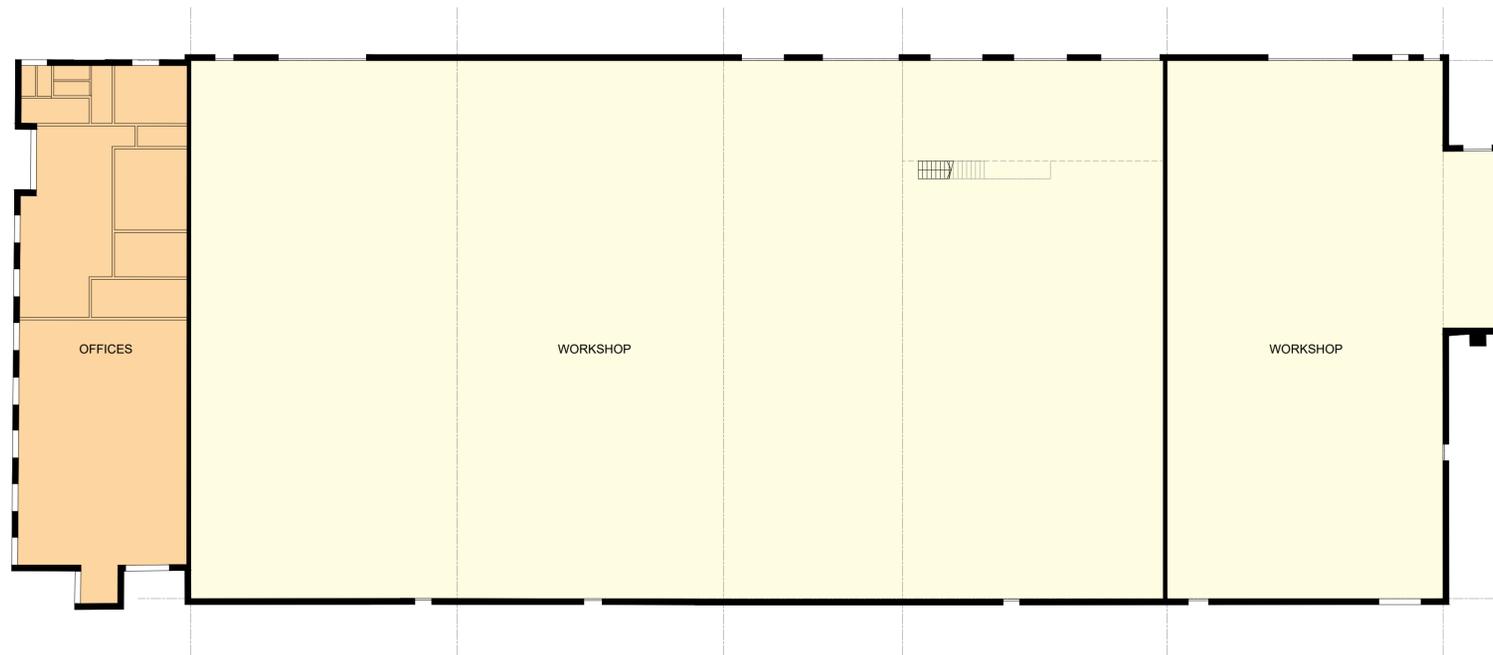
Issue Status:
Construction Preliminary
Information Approval
Tender

Offices
Woking
London
Milton Keynes
Warsaw

PRC Architecture & Planning



FIRST FLOOR PLAN



GROUND FLOOR PLAN



Client:
VAMOS INVEST LTD



Project:
HOLMETHORPE AVENUE, REDHILL

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246

info@prc-group.com
www.prc-group.com

Drawing Title:
GROUND FLOOR AND FIRST FLOOR
EXISTING SCHEME

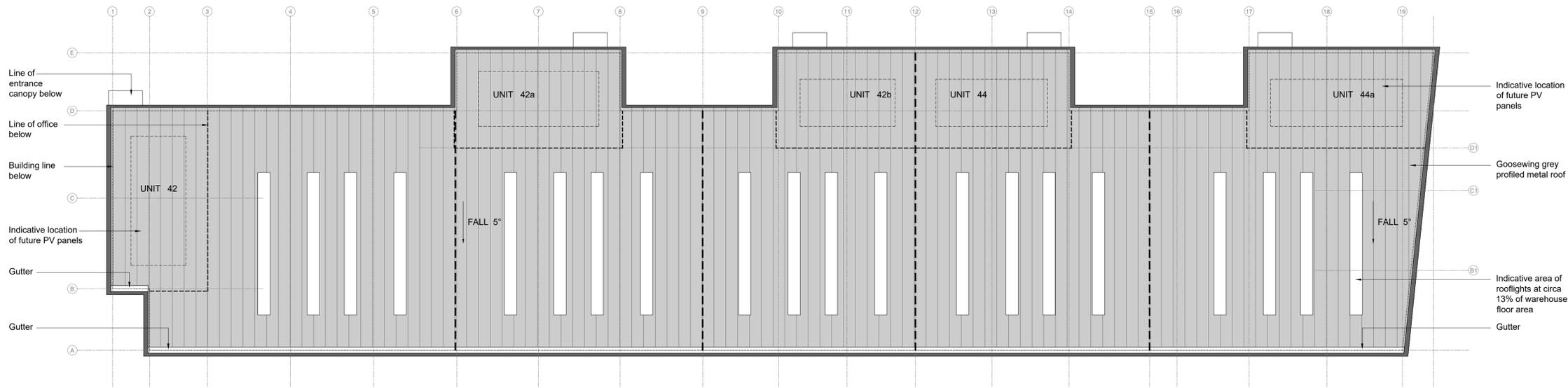
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Date: APR 21

Job No: 11361
Stage: PL
Drawing No: 021
Rev:

Issue Status:
Construction Preliminary
Information Approval
Tender

Architecture
Planning
Master Planning
Urban Design
Interiors
Landscape

Offices
Woking
London
Milton Keynes
Warsaw

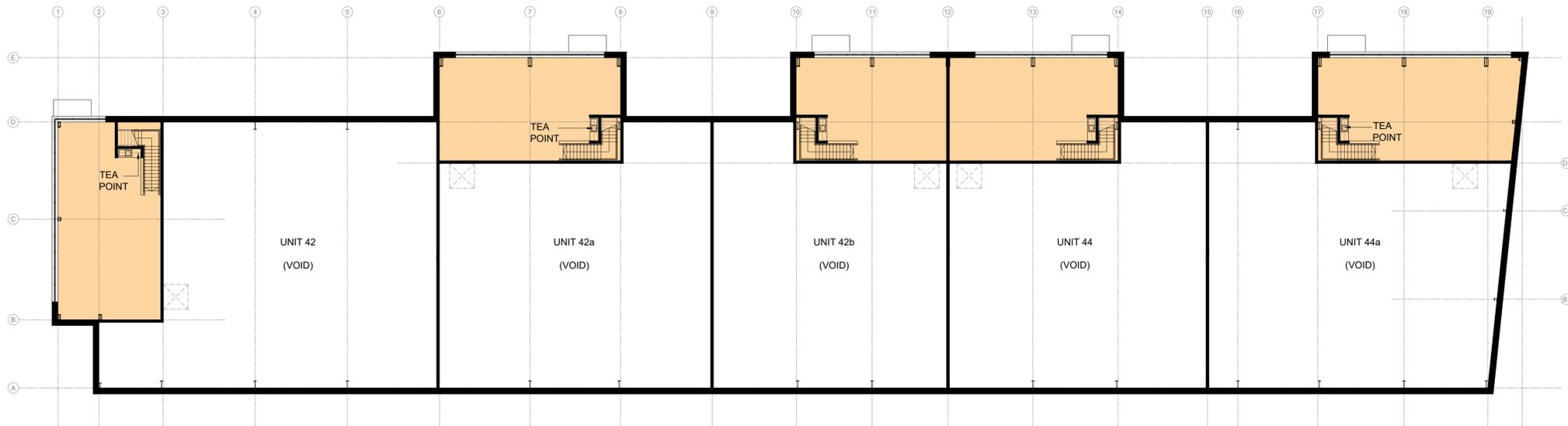


ROOF PLAN

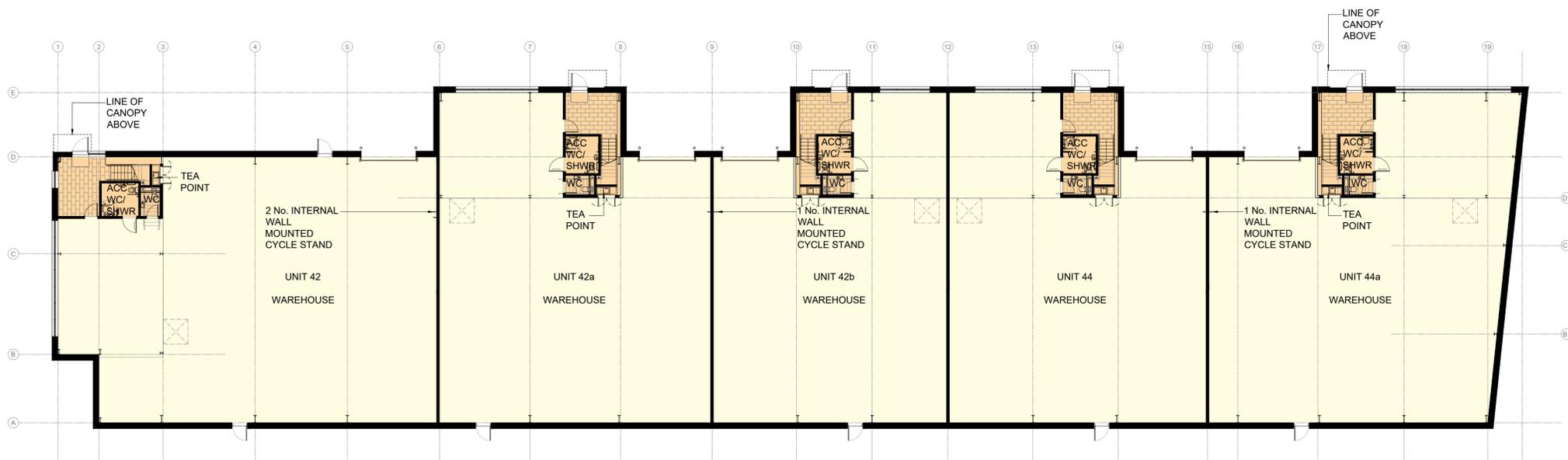


GEA			
	Ground (m ²)	First (m ²)	Total (m ²)
1.	553.24	122.16	675.40
2.	468.69	111.95	580.64
3.	400.88	92.43	493.31
4.	442.22	103.12	545.34
5.	514.21	123.56	637.77
T	2,379.24	553.22	2,932.46

GIA			
	Ground (ft ²)	First (ft ²)	Total (ft ²)
1.	5,990	1,155	6,745
2.	4,785	1,060	5,845
3.	4,090	880	4,970
4.	4,520	990	5,510
5.	5,180	1,165	6,345
T	24,165	5,250	29,415



FIRST FLOOR PLAN



GROUND FLOOR PLAN



Client:
VAMOS INVEST LTD



Project:
HOLMETHORPE AVENUE, REDHILL

12 Warren Yard,
Warren Park,
Milton Keynes.
MK12 5HW
01908 305 246

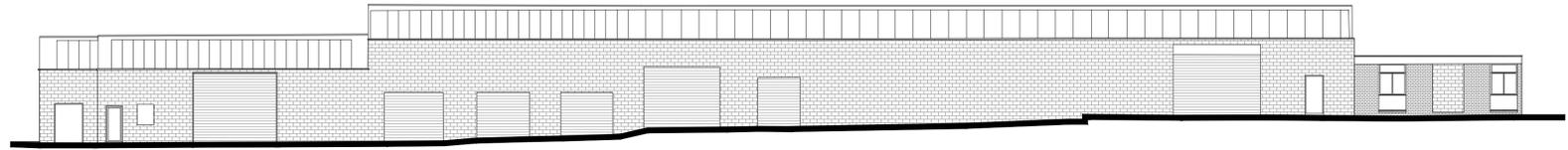
info@prc-group.com
www.prc-group.com

Drawing Title:
GROUND FLOOR, FIRST FLOOR
AND ROOF PLAN

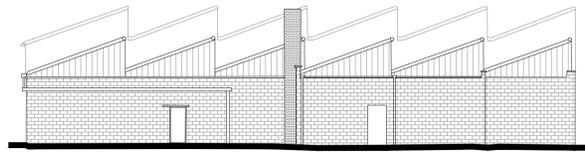
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Checked by: JR
Date: APR 21

Job No: 11361
Stage: PL 003
Drawing No: PL 003
Rev: _____

Issue Status:
 Construction Preliminary
 Information Approval
 Tender _____
Offices
 Woking
 London
 Milton Keynes
 Warsaw



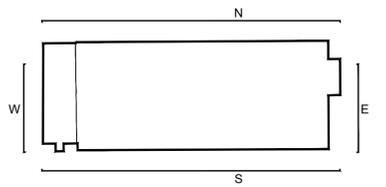
NORTHERN ELEVATION



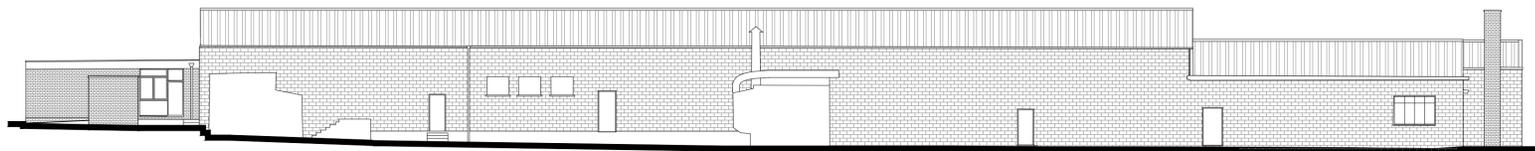
EASTERN ELEVATION



WESTERN ELEVATION



BASED ON SURVEY DRAWINGS
BY LHL GROUP
DRAWING NUMBERS:
Y-F-4927-15-02
Y-F-4927-15-03



SOUTHERN ELEVATION



Client:
VAMOS INVEST LTD



Project:
HOLMETHORPE AVENUE, REDHILL

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246

info@prc-group.com
www.prc-group.com

Drawing Title:
EXISTING ELEVATIONS

Scale @ A1: 1:200
Checked by: JR
Date: APR 21

Job No: 11361
Stage: PL 022
Drawing No: PL 022
Rev:

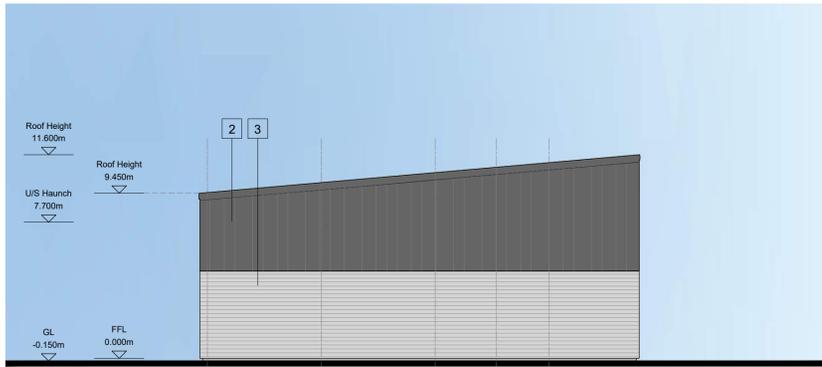
Issue Status:
Construction Preliminary
Information Approval
Tender

Architecture
Planning
Master Planning
Urban Design
Interiors
Landscape

Offices
Woking
London
Milton Keynes
Warsaw



NORTHERN ELEVATION

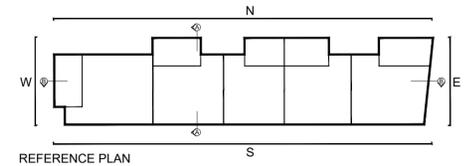


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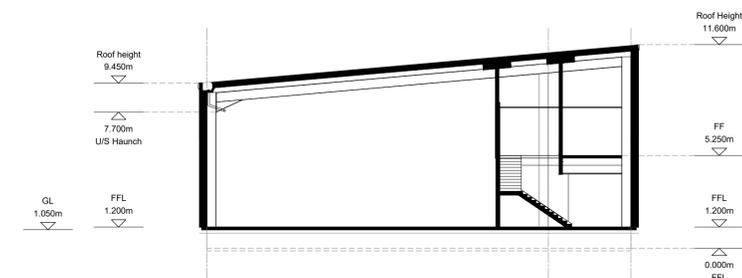


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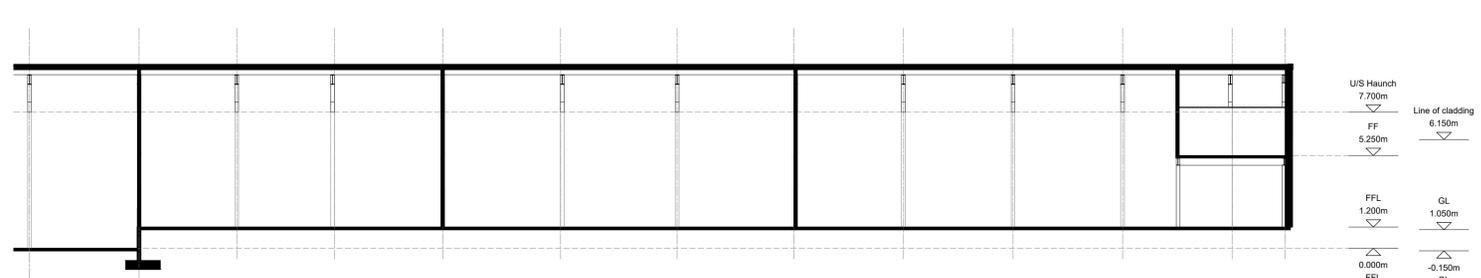
- MATERIALS KEY**
- 1 PROFILED METAL ROOF CLADDING GOOSEWING GREY
 - 2 MICRORIB COMPOSITE CLADDING ANTHRACITE
 - 3 HORIZONTAL PROFILE CLADDING SILVER GREY
 - 4 ALUMINIUM FRAMED WINDOWS ANTHRACITE
 - 5 ALUMINIUM FRAMED ENTRANCE DOORS ANTHRACITE
 - 6 FEATURE ENTRANCE PANEL TIMBER CLADDING
 - 7 LOADING BAY DOORS ANTHRACITE
 - 8 STEEL PERSONNEL DOORS ANTHRACITE
 - 9 STEEL BOLLARD
 - 10 MAIN ENTRANCE GLAZED CANOPY
 - 11 BACK LIT UNIT NUMERALS SILVER
 - 12 SIGNAGE ZONE
 - 13 OBSCURE GLASS PANELS



SOUTHERN ELEVATION



TYPICAL CROSS SECTION A - A



LONG SECTION B - B



Client: VAMOS INVEST LTD



Project: HOLMETHORPE AVENUE, REDHILL

12 Warren Yard, Warren Park, Milton Keynes, MK12 5HW, 01908 305 246

info@prc-group.com www.prc-group.com

Drawing Title: PROPOSED ELEVATIONS AND SECTIONS

Scale @ A1: 1:200

Checked by: JR

Date: APR 21

Job No: 11361

Stage: PL 004

Issue Status:

Construction Preliminary

Information Approval

Tender

Offices: Working London, Milton Keynes, Warsaw

PRC Architecture & Planning



SECTION A-A
PROPOSED STREET ELEVATION



SECTION B-B
PROPOSED STREET ELEVATION

0 5 10 15 20 25m
Scale 1:250



Client:
VAMOS INVEST LTD



Project:
HOLMETHORPE AVENUE, REDHILL

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246

info@prc-group.com
www.prc-group.com

Drawing Title:
STREET SCENES

Scale @ A1: 1:250
Checked by: JR
Date: APR 21

Job No: 11361
Stage: PL 005
Drawing No: _____
Rev: _____

Issue Status:
Construction Preliminary
Information Approval
Tender

Architecture
Planning
Master Planning
Urban Design
Interiors
Landscape

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Woking
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1st September 2021

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 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	1st September 2021
	REPORT OF:	HEAD OF PLACES & PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	8	WARD: Horley Central and South

APPLICATION NUMBER:	21/00527/F	VALID:	25.03.2021
APPLICANT:	Mr James Smith	AGENT:	Grainger Planning Associates Ltd
LOCATION:	17 CHURCH ROAD HORLEY SURREY RH6 7EY		
DESCRIPTION:	Demolition of the existing dwelling; erection of a development of six flats in a two storey building with roof accommodation together with the provision of refuse and recycling stores, 8 car parking spaces and new access. As amended on 06/08/2021.		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

This is a full application to demolish the existing building, and to erect a two-storey building with roof accommodation. The building will accommodate six 2-bed 3-person flats. The front elevation would have a double gabled frontage. The rear elevation would also have a double gable, but would sit at a lower level. The predominant materials would include plain clay roof tiles, handmade plain clay tile hanging and stock brickwork. The bay windows would be clad in timber boarding. The windows would be timber. The gables would include mock Tudor gable detail.

The site is currently occupied by a two storey detached building which under the last planning application was stated to be in use as a Guest House. The property has a hipped roof with two pitched roof gables to the principal elevation, offering a quite traditional villa appearance. The site is set within a long rectangular plot and remains flat throughout. The front of the site is currently almost entirely laid to hardstanding.

The area is predominantly residential in character in one form or another, with a number of guest houses and hotels within the wider area. To the immediate east of the site are two large, flatted buildings. To the south and west are smaller scale detached, semi-detached, and terrace dwelling houses within Pine Gardens and the recent Astor Gardens development. Further to the south, adjoining the rear

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boundaries of Astor Gardens and Pine Gardens is the Massetts Road Conservation Area

Overall, it is considered that the proposed development would be of appropriate scale and design and would not be unduly detrimental to the street scene of Church Road or the character of the wider locality and would cause no material harm to the setting of the Massetts Road Conservation Area.

The proposal provide an acceptable level of amenity for future occupants. The proposal would not have a significant adverse effect upon existing neighbouring properties.

The proposal would provide parking in line with the DMP parking standards and would, subject to conditions, be acceptable with regard to the impact on ecology, landscaping and sustainable construction.

The proposals would make efficient use of this previously developed site for new housing and would accord with the Development Plan

RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

Consultations:

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to condition.

Neighbourhood Services: Requested that the bin area was moved to the front of the car park.

Horley Town Council: No objection

Gatwick Aerodrome Safeguarding: No objection. Observation regarding cranes provided.

Representations:

Letters were sent to neighbouring properties on 26/3/2021 and 20/4/2021.

No responses have been received.

1.0 Site and Character Appraisal

1.1 The site is currently occupied by a two storey detached building which under the last planning application was stated to be in use as a Guest House. The property has a hipped roof with two pitched roof gables to the principal elevation, offering a quite traditional villa appearance. The site is set within a long rectangular plot and remains flat throughout. The front of the site is currently almost entirely laid to hardstanding.

1.2 The area is predominantly residential in character in one form or another, with a number of guest houses and hotels within the wider area. To the immediate east of the site are two large, flatted buildings. To the immediate south and west are smaller scale detached, semi-detached, and terrace dwelling houses within Pine Gardens and the recent Astor Gardens development. Further to the south, adjoining the rear boundaries of Astor Gardens and Pine Gardens is the Massetts Road Conservation Area. Further to the west are larger scale villa properties in residential and guest house uses.

2.0 Added Value

2.1 Improvements secured at the pre-application stage: The applicant entered into pre-application discussions with the Council for 8 flats (PAW/20/00411). Advice was provided regarding the scale and design of the building, neighbouring amenity, and amenity of future occupants and parking. It was advised that the number of units should be reduced to reduce scale of the building and it should better reflect the existing villa style of the current building.

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2.2 Improvements secured during the course of the application: an additional parking space has been added and there have been changes to the roof form – lowering of the eaves and increase in pitch of the roof.

2.3 Further improvements to be secured through the use of conditions in relation to materials, landscaping, ecology enhancements, sustainable construction and disabled parking

3.0 Relevant Planning and Enforcement History

3.1 19/00064/F Two storey rear and side extension Granted
14 June 2019

4.0 Proposal and Design Approach

4.1 This is a full application to demolish the existing building, and to erect a two-storey building with roof accommodation. The building will accommodate six 2-bed 3-person flats. 6 parking spaces and a bin store are proposed to the front of the site, 2 parking spaces and cycle storage are proposed to the rear of the site. Soft landscaping is shown to the front and side boundaries. A communal outdoor amenity space is proposed to the rear.

4.2 The front elevation would have a double gabled frontage. The rear elevation would also have a double gable, but would sit at a lower level. The predominant materials would include plain clay roof tiles, handmade plain clay tile hanging and stock brickwork. The bay windows would be clad in timber boarding. The windows would be timber. The gables would include mock Tudor gable detail.

4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.

4.4 Evidence of the applicant's design approach is set out below:

Assessment	The character of the surrounding area is assessed at section 2 of the Planning, Design and Access Statement. The building is identified as late-Edwardian and similar to the other larger former villas within the surrounding area. However recent re-development and back land development has altered the character of the area with a lack of uniformity and architectural style.
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	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The other development options considered were those discussed at pre-application stage which are set out within the design statement
Design	The applicant's reasons for choosing the proposal were a response to pre-application with it reflecting more closely the Edwardian origins of the area and materials while giving it a more contemporary feel.

4.5 Further details of the development are as follows:

Site area	0.07ha
Existing use	Guest House
Proposed use	C3 - residential
Existing parking spaces	10
Proposed parking spaces	8
Parking standard	8
Net increase in dwellings	5
Existing site density	N/A – guest house
Proposed site density	85dph
Density of the surrounding area	93 dph – 15 and 2 Pine Gardens 45 dph – 4-32 Pine Gardens 24dph – 1-11 Astor Gardens

5.0 Policy Context

5.1 Designation

Urban area
Setting of Massetts Road Conservation Area
Parking standards: Medium accessibility

5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)
CS4 (Valued Townscapes and Historic Environment)
CS5 (Valued People/Economic Development),
CS10 (Sustainable Development),
CS11 (Sustainable Construction),
CS17 (Travel Options and accessibility)

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5.3 Reigate & Banstead Development Management Plan 2019

Design, Character and Amenity (including housing)	DES1, DES4, DES5, DES8
Landscape & Nature Conservation	NHE2, NHE3
Heritage	NHE9
Infrastructure	INF3
Transport, Access and Parking	TAP1
Climate Change Resilience and Flooding	CCF1, CCF2

5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Surrey Design Local Character and Distinctiveness Design Guide 2021 Vehicle and Cycle Parking Guidance 2018 Householder Extensions and Alterations
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010 Conservation of Habitats and Species Regulations 2010

6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Highway matters
- Sustainable construction
- Ecology and landscaping
- Drainage and flooding
- Community Infrastructure Levy
- Infrastructure contributions

Design appraisal

- 6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.4 The existing building is attractive but not a statutory or locally listed building and is located outside of a Conservation Area. There is therefore no objection to its demolition.
- 6.5 In terms of the replacement building it is considered that the scale of the building, whilst large, would not be out of keeping with the scale of the buildings within the surrounding area. Particularly the flatted buildings to the east of the site and the larger detached properties to the west of the site where the depth and width of the proposed building would be commensurate with these buildings. The depth of the proposed building would also be very similar to the depth of the extant 2019 permission, which granted a 5.6m two storey rear extension.
- 6.6 The street scene plans show that the height of the building would be in keeping with those of the neighbouring properties along Church Road. In terms of its position within the site it would maintain the stepped front building line which is a character of the road. The spacing to the boundary would be minimal at around 1 metre but again this is considered to be in keeping with the general character of the road, where there are large buildings with limited spacing to the boundaries.
- 6.7 In terms of the appearance of the proposed building. It is considered that the design would reflect the Edwardian origins of the road with the use of large symmetrical double gables and projecting bays with the use of predominantly traditional materials and finishes whilst also providing more contemporary features such as the timber cladding and larger glazed windows. During the course of the application the eaves height of the roof has been lowered to better reflect the eaves height and pitch of the remaining Edwardian villas along Church Road. Such an approach would ensure that the building does not appear out of keeping with the general form and character of the surrounding area.
- 6.8 The proposal would also result in a significant improvement to the appearance of the front of the site due to the reduction in the amount of hardstanding proposed to the front of the site. This has created space for soft landscaping which will enhance the character of the site. It is worth

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noting that under the extant 2019 permission the front of the site, and majority of the rear of the site was to be hardstanding. In this case landscaping is proposed throughout the site.

- 6.9 As set out above the site is located to the north of the Massetts Road Conservation Area. The Council's Conservation Officer has advised "I think that the building is sufficient distance from the existing Conservation Area not to affect it." It is therefore considered that the proposal would not result in a material impact on the Conservation Area. The Conservation Officer did raise concerns regarding the eaves lines of the building and the scale of the building. However, for the reasons set out above officers are of the view that the scale and appearance of the building is acceptable and would not result in detrimental harm to the character of the site or surrounding area.
- 6.10 DMP Policy DES4 relates to Housing Mix and states that all new residential developments should provide homes of an appropriate type, size and tenure to meet the needs of the local community. The proposed housing mix must on sites of up to 20 homes, at least 20% of market housing should be provided as smaller (one and two bedroom) homes. In this case, the proposal provides 100% smaller units.
- 6.11 DMP Policy DES5 relates to the delivery of high quality homes and requires, inter alia, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units except where the Council accepts that an exception to this should be made in order to provide an innovative type of affordable housing that does not meet these standards. In addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.
- 6.12 Each dwelling would have a floor area which accords with the relevant standard in the Nationally Described Space Standards. Each flat has a private balcony or terrace area. All of the proposed sitting rooms would have dual aspect windows and the only bedrooms with aspect to the side of the site would be those which have outlook on to the access road to Astor Gardens. As such the units would provide good levels of sunlight and daylight to the main habitable rooms and adequate outlook. The flats would also benefit from outdoor communal amenity space to the rear of the site. Given that the flats are 2 bedroom units this level of outdoor amenity space is considered to be acceptable.
- 6.13 Overall, it is considered that the proposed development would be of appropriate scale and design and would not be unduly detrimental to the street scene of Church Road or the character of the wider locality and would provide an acceptable level of amenity for future occupants. It therefore complies with policies DES1, DES4 and DES5 in this respect.

Neighbour amenity

- 6.14 The nearest dwellings to the south of the site would be 12 Pine Gardens to the south-east and 1 Astor Gardens to the south. These dwellings would be approximately 17m and 26m respectively from the rear most part of the proposed dwelling. Given the separation distance and rear to side elevation relationship it is considered that the proposal would not result in an unacceptable impact on neighbouring amenity in terms of loss of privacy, overbearing impact and loss of light.
- 6.15 To the east of the site is no.15 Church Road. The proposed building would extend beyond the rear elevation however due to the separation distance the proposal would pass the 45 degree test in relation to the impact on the rear facing windows of no.15 and there would not be an unacceptable impact in terms of overbearing impact or loss of light. There are side facing windows at no.15 which serve bedrooms. However, the outlook and light to these windows is already impacted by the existing building. As such the proposal would not result in a significantly worse situation. There are a number of side facing windows proposed. It is recommended to condition that the side facing windows to the east elevation at first floor level are obscure glazed and restricted opening to protect the privacy of the occupants at no.15. The ground floor windows would be obscured by the boundary fence and the rooflights are at relatively high level (cill 1.5m above floor level) preventing any unacceptable outlook.
- 6.16 To the west of the site is the access road to the Astor Gardens development. Beyond is 2 Astor Gardens. The proposed building would extend beyond the rear of no.2 however the proposal would pass the 45 degree test and due to the separation distance, the property's orientation which has main outlook to north and south, and commensurate height of the buildings it is not considered that the proposal would not result in unacceptable loss of light or overbearing impact to no.2. There are side facing windows at no.2. According to planning records these windows are secondary at the ground floor, serve non habitable rooms at first floor and one bedroom dormer window at second floor level. The proposed building does include side facing windows. However these are all secondary except for the bedroom windows to flat 2, 4 and 6. These windows are at the northern end of the building which would not allow direct outlook towards the windows which serve habitable rooms at no.2. The plans state that the remainder of the first floor windows would be obscure glazed and non-opening. A condition is recommended to secure this. The ground floor windows would be obscured by the boundary fence and the rooflights are at relatively high level (cill 1.5m above floor level) and angled preventing any unacceptable outlook.
- 6.17 There would be no material change to the relationship with the buildings to the north of the site.
- 6.18 The site is located within close proximity of a number of residential properties. To reduce the impact on neighbouring residents were the application to be approved a condition is recommended to secure the submission of a Construction Management Statement which addresses matters such as working hours and potential disruption from noise and pollution.

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- 6.19 In conclusion, the proposal would not have a significant adverse effect upon existing neighbouring properties and would accord with the provisions of DMP Policy DES1 and DES8.

Highway matters

- 6.20 The proposed development has been reviewed by the County Highway Authority (CHA), with regard to highway safety, capacity and policy matters, who raise no objections subject to the imposition of a number of conditions.
- 6.21 The site is located in an area which is assessed as having a medium accessibility rating. In such areas, the Council's adopted parking standards require the provision of 1 space for each of the 2 bedroom flats plus 2 visitor parking spaces. Thus, a total of 8 spaces would be required. In this case, a total of 8 spaces are proposed in line with the minimum standards. It is noted that two spaces are located to the rear of the site and gain access via Astor Gardens. The applicant has advised that the applicant has an established right of way to the rear of the site and therefore the rear spaces will be accessible by residents. A six Sheffield stand cycle store would be provided in line with the requirement for a minimum of 1 per unit.
- 6.22 Concern has been raised by members that the parking layout does not include a disabled parking space. It is considered that there is adequate space within the site to increase the size of one of the spaces to accommodate a disabled space and this could be secured by condition were the application to be approved.
- 6.23 The CHA did initially request that the applicant agree to provide a footway to the front of the site. However, on review the CHA has advised that as the site was formerly in multiple occupation as a Guest House that there is no justification for this footway.
- 6.24 Accordingly, it is considered that the proposals are acceptable from a highway point of view and accord with the provisions of DMP Policy TAP1.

Sustainable construction

- 6.25 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. Whilst the application submission does not detail how this requirement would be met in the event that planning permission is to be granted, a condition could be imposed to seek such information and its implementation prior to the first occupation of development. In this regard, there would be no conflict with DMP Policy CCF1.

- 6.26 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.

Ecology and landscaping

- 6.27 The site is located within the urban area and there are no trees within the site or close to the site. It is therefore considered that the site is of low ecological value and the proposal is unlikely to result in harm to protected species. There is however the opportunity for the proposal to provide biodiversity enhancement measures in line with policy NHE2. Were the application approved a condition is recommended to secure further details and implementation.
- 6.28 The proposal shows a good level of new soft landscaping. At this stage details regarding the exact species and location of planting is not provided. Were the application to be approved a condition is recommended to secure further details and implementation.

Drainage and flooding

- 6.29 The site is not within Flood Zones 2 or 3 and is considered to have a satisfactory impact with regard to fluvial flooding. Sewage capacity would be dealt with under Building Regulations. To ensure that the site is satisfactorily drained with regard to Development Management Plan policy CCF2 and National Planning Policy Framework 2019, a condition is recommended to secure details of a scheme for the disposal of foul and surface water drainage from the site prior to commencement of development

Community Infrastructure Levy (CIL)

- 6.30 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and the exact amount would be determined and collected after the grant of planning permission.

Infrastructure Contributions

- 6.31 In terms of other contributions and planning obligations, The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified. Accordingly,

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any request for an infrastructure contribution would be contrary to CIL Regulation 122.

CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Combined Plan	P1		03.03.2021
Existing Plans	440-01		25.03.2021
Floor Plan	P3	C	06.08.2021
Site Layout Plan	P2	C	06.08.2021
Elevation Plan	P4	C	06.08.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (e) provision of visibility zones behind any boundary hoarding.

(h) measures to prevent the deposit of materials on the highway has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019, Reigate and Banstead Core Strategy 2014 Policy CS17 and Reigate and Banstead Development Management Plan September 2019 policies TAP1 and DES8.

5. No development shall commence until a Construction Management Statement, to include details of:
- (a) Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
 - (b) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
 - (c) Means of communication and liaison with neighbouring residents and businesses.
 - (d) Hours of work.
- Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

6. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:
- (a) Evidence that there is no risk of contamination through the infiltration SuDs
 - (b) Evidence that the proposed final solution will effectively manage the 1 in 30 and 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development (Pre, Post and during), associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 37.8l/s.
 - (c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels and long and cross sections of each element

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- including details of any flow restrictions and maintenance / risk reducing features (silt traps, inspections chambers etc)
- (d) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational
 - (e) Details of drainage management responsibilities and maintenance regimes for the drainage system
 - (f) A plan showing exceedance flows (ie during rainfall greater than design events or during blockage) and how property on and offsite will be protected. This should include details of how surface water run-off entering the site from the bunded northern boundary will be intercepted.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

7. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

8. No development above slab level shall commence on site until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development

or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or any existing plants/hedging retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and Meath Green Conservation Area, and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

9. No development above slab level shall commence until a scheme to provide positive biodiversity benefits, has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2

10. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

11. The development hereby approved shall not be first occupied unless and until the proposed vehicular access to Church Road has been constructed in accordance with the approved plans.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other

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highway users, and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

12. The development hereby approved shall not be occupied unless and until any redundant sections of the existing access from the site to Church Road have been permanently closed and any kerbs, verge, footway, fully reinstated.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1.

13. The development hereby approved shall not be first occupied unless and until an updated parking layout has been submitted to and approved in writing by the Local Planning Authority to show one of the spaces as a disabled space. The spaces shall be laid out within the site in accordance with the approved plans prior to the first occupation of the building for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking /turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

14. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for bicycles to be stored. Thereafter the cycle parking area shall be retained and maintained for its designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

15. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall include wildlife friendly access and be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3.

16. The refuse store shall be erected and made ready for use (i.e. bins installed) in accordance with the approved plans prior to the first occupation of the development.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

17. The development hereby approved shall not be occupied unless and until a minimum of two of the available parking spaces are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter retained and maintained for their designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to preserve the character of the Conservation Area, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 and NHE9 of the Development Management Plan.

18. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
 - Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

19. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- A broadband connection accessed directly from the nearest exchange or cabinet
 - Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

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20. Notwithstanding the approved plans all of the first floor windows in the east and west side elevations of the development hereby permitted, with the exception of the bedroom window to flat 4 on the west elevation, shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. The Council's Neighbourhood Services team can be contacted on 01737 276292 or via the Council's website at http://www.reigate-banstead.gov.uk/info/20085/planning_applications/147/recycling_and_waste_developers_guidance
3. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

4. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
5. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering
6. The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to submit to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land

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Drainage Act 1991. Please see: www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice.

7. When a temporary access is approved or an access is to be closed as a condition of planning permission an agreement with, or licence issued by, the Highway Authority Local Highways Service will require that the redundant dropped kerb be raised and any verge or footway crossing be reinstated to conform with the existing adjoining surfaces at the developers expense.
8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
9. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
10. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
11. The applicant is advised that the development should seek to achieve standards contained within the Secured by Design award scheme to ensure that it results in a safe development.
12. Cranes – if cranes are used during construction the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks notice. For crane queries/applications please email: lgwcranes@gatwickairport.com

REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS5, CS10, CS11, CS17, DES1, DES4, DES5, DES8 NHE2, NHE3 NHE9 INF3 TAP1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

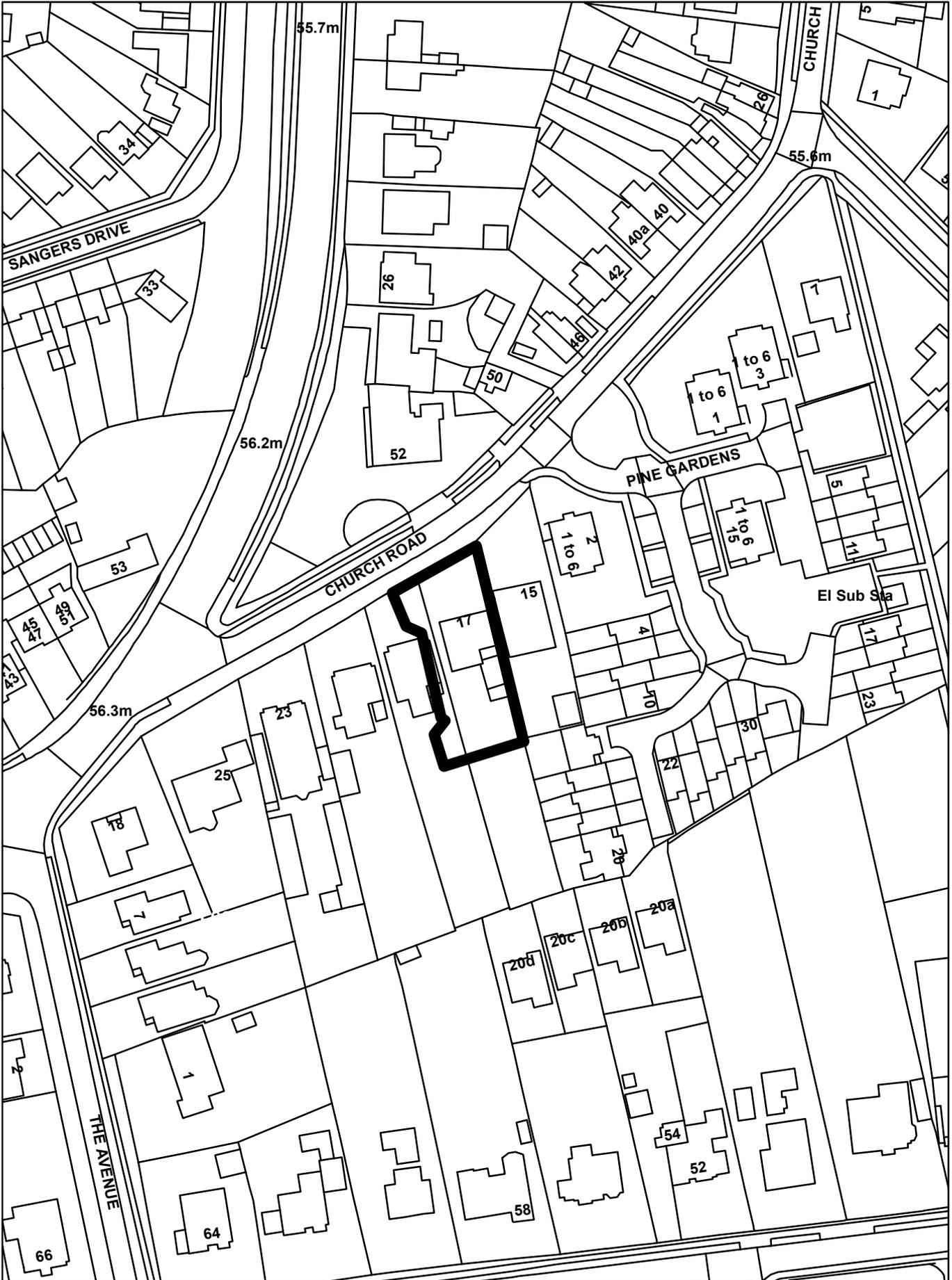
Agenda Item 8

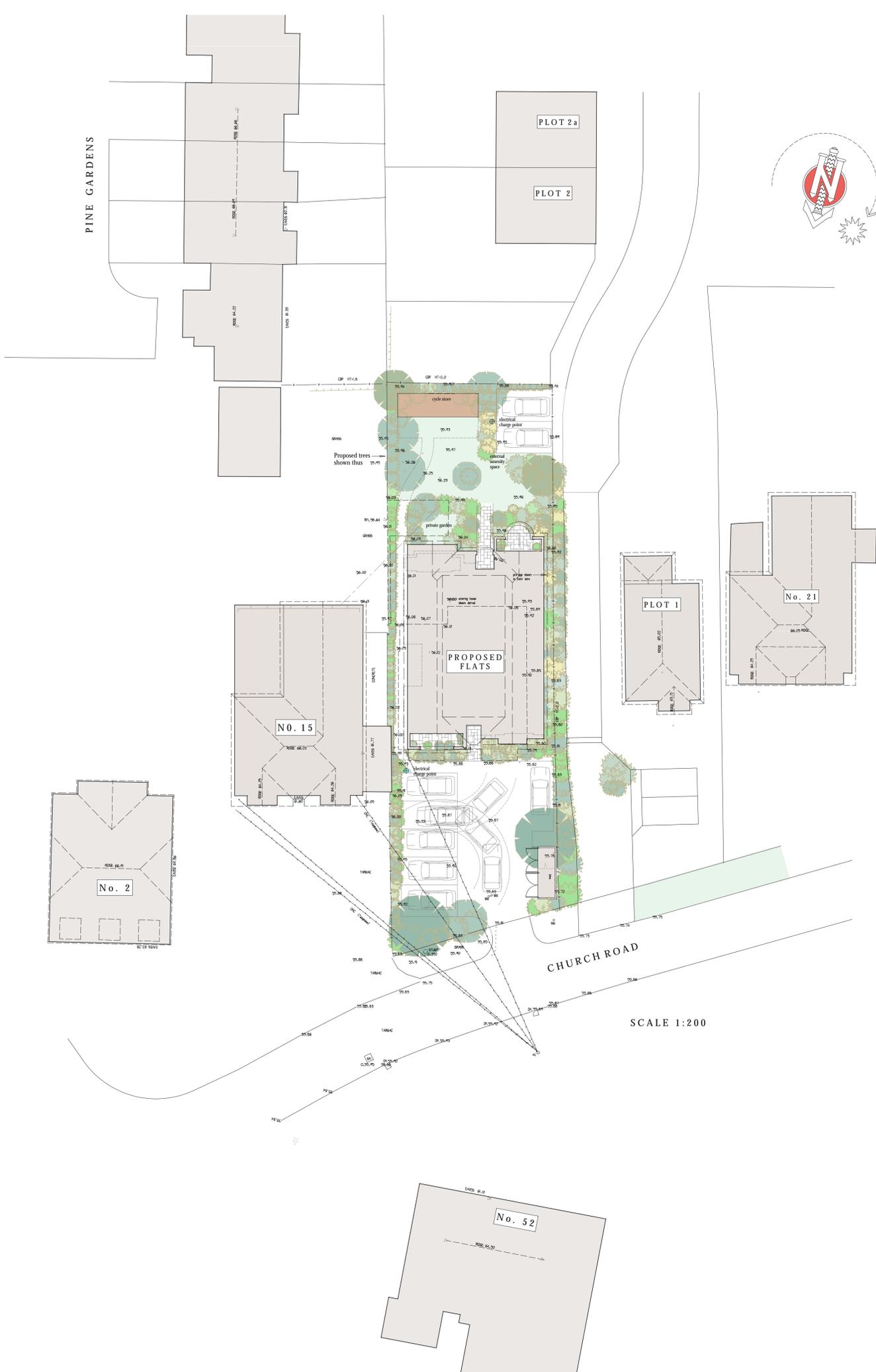
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The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

17 Church Road Horley Surrey RH6 7EY

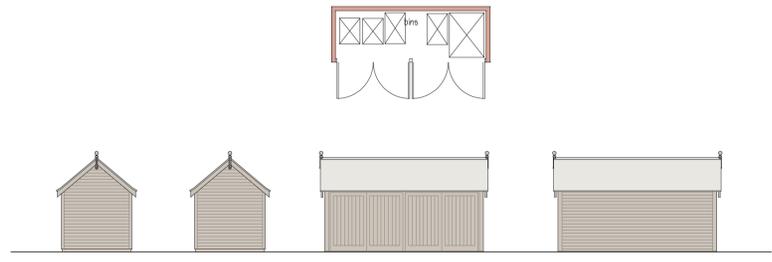




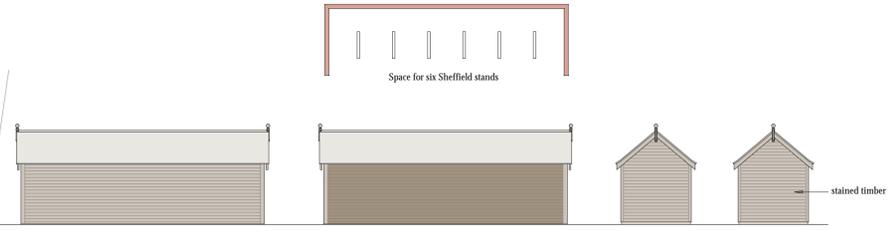
2 PINE GARDENS 15 CHURCH ROAD 17 CHURCH ROAD PLOT 1 of NEW DEVPT.
PROPOSED STREET SCENE SCALE 1:100



2 PINE GARDENS 15 CHURCH ROAD 17 CHURCH ROAD PLOT 1 of NEW DEVPT.
PRE-APP STREET SCENE SCALE 1:100



END ELEVATIONS FRONT ELEVATION REAR ELEVATION
BIN STORE



REAR ELEVATION FRONT ELEVATION END ELEVATIONS
CYCLE SHED SCALE 1:100



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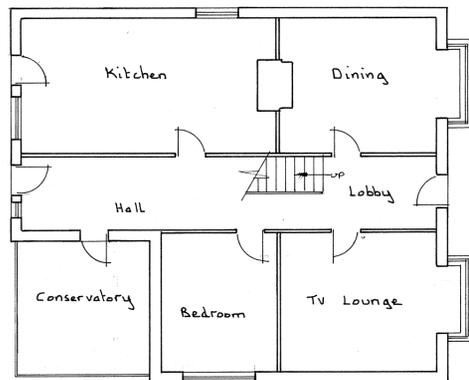
Proposed Development
 at
17 CHURCH ROAD
HORLEY RH6 7EY
 for Nutfield Homes Ltd

PROPOSED
SITE PLAN

scale 1:200 02/02/2021

Rev C 25/07/2021 Additional parking and a bin store
 Rev B 07/02/2021 cycle shed shown
 Rev A 06/02/2021 Parking revised

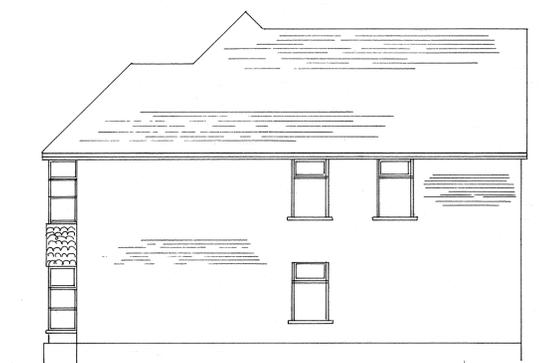
17CR **P2 (C)**



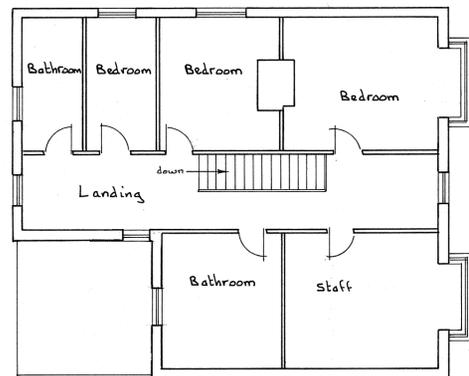
Ground Floor Plan



South Elevation



West Elevation



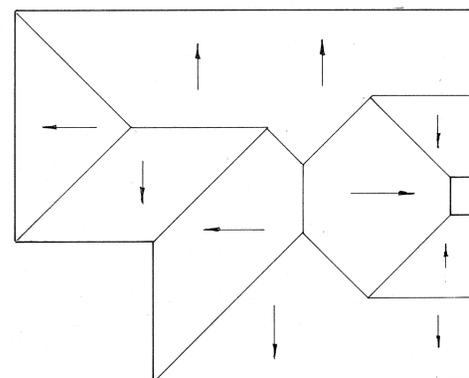
First Floor Plan



East Elevation



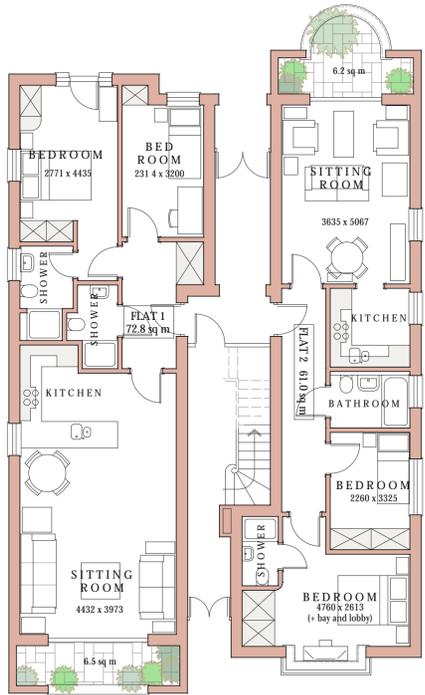
North Elevation



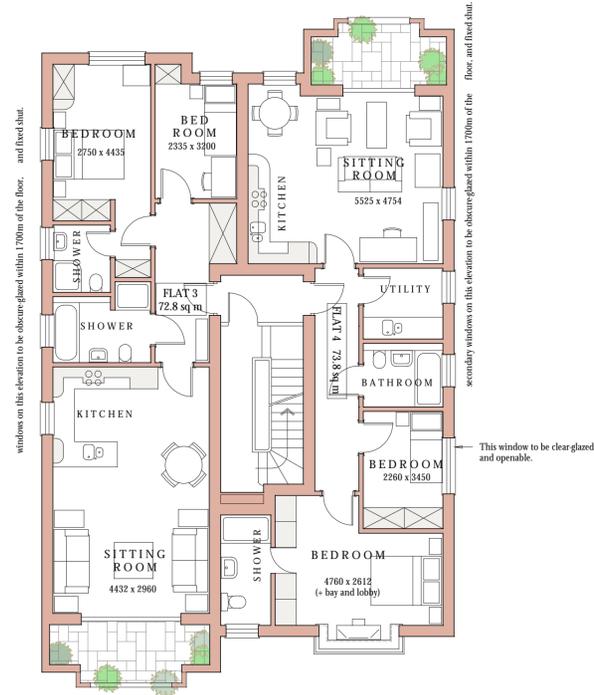
Roof Plan



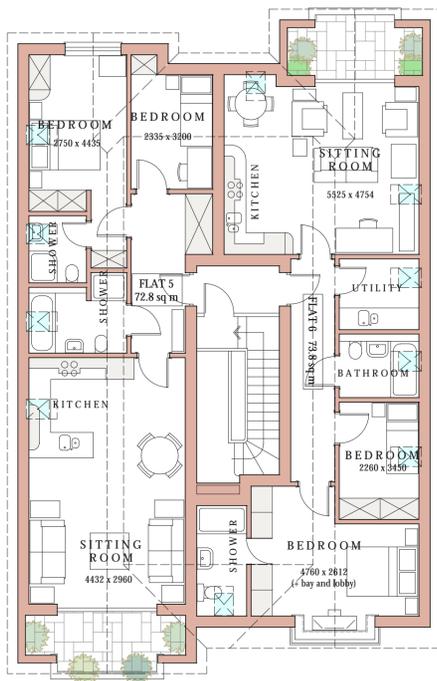
17 Church Rd. Horley RH6 7EY
Proposed Two Storey Rear/Side Extension
Existing Layout
Scale:- 1/100 unless stated otherwise
Date:- Dec. 2018 Drg. N^o. 640-01



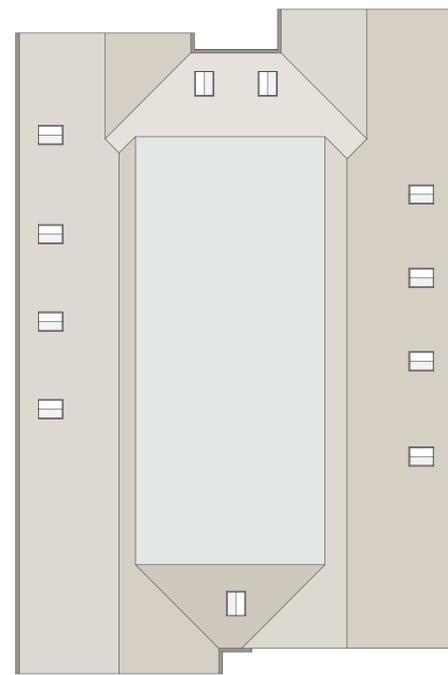
GROUND FLOOR PLAN



FIRST FLOOR PLAN



SECOND FLOOR PLAN



SECOND FLOOR PLAN

- Six 2-bed flats proposed
- FLAT 1: 72.8 sq m
 - FLAT 2: 61.0 sq m
 - FLAT 3: 72.8 sq m
 - FLAT 4: 73.8 sq m
 - FLAT 5: 72.8 sq m
 - FLAT 6: 73.8 sq m



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email: graham@grahamrix.co.uk

Proposed Development
at
17 CHURCH ROAD
HORLEY RH6 7EY
for Nutfield Homes Ltd

PROPOSED
FLOOR PLANS

scale 1:100 02/02/2021

Rev C 25/07/2021
Rev B 07/02/2021
Rev A 06/02/2021

17CR P3 (C)



FRONT ELEVATION

SCALE 1:50



REAR ELEVATION



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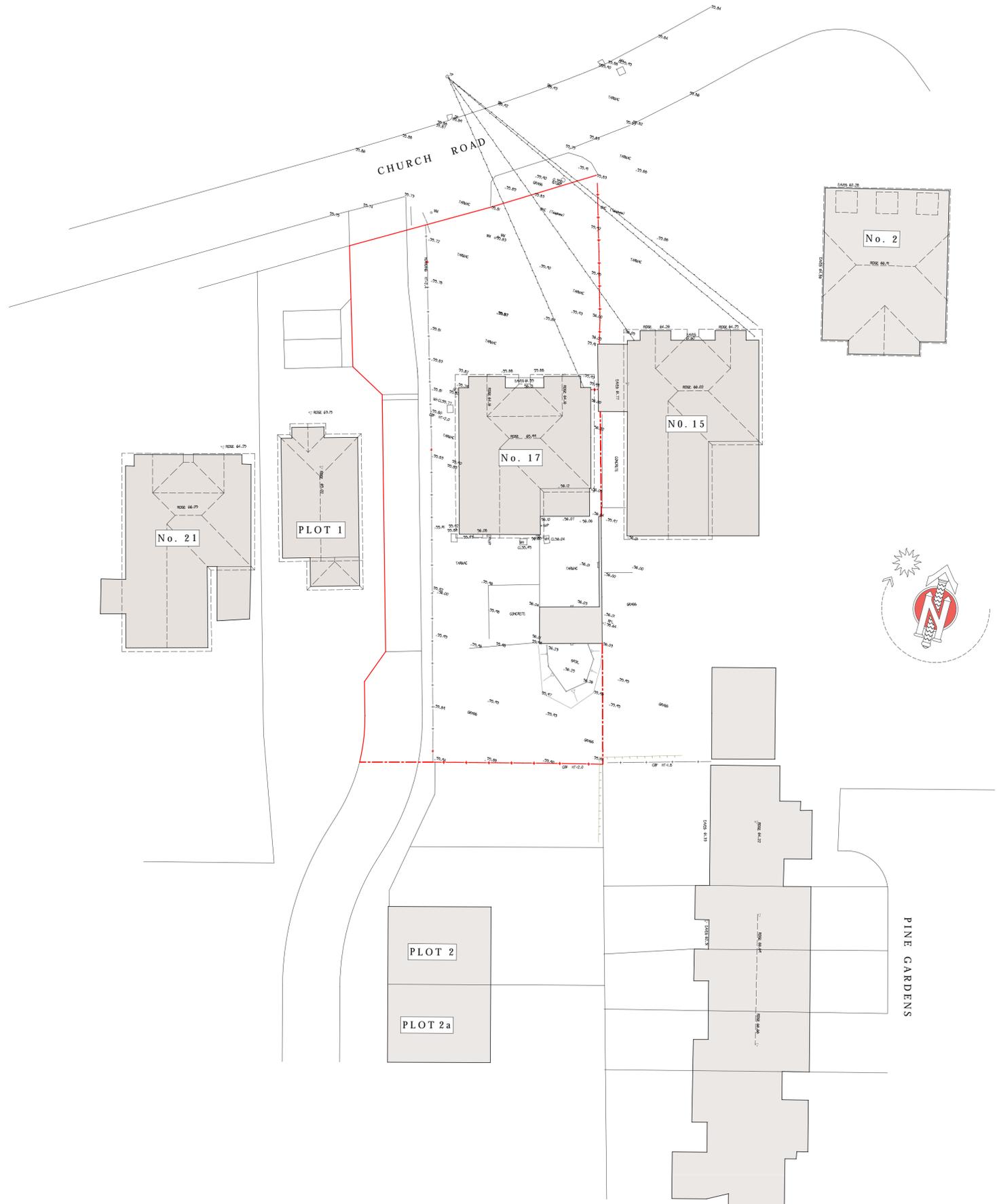
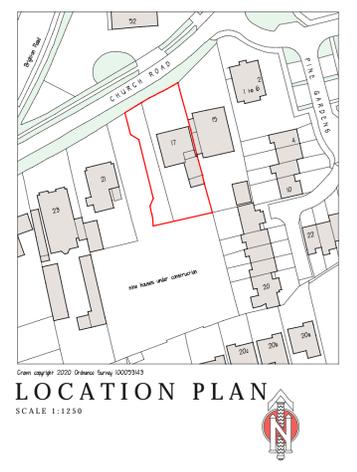
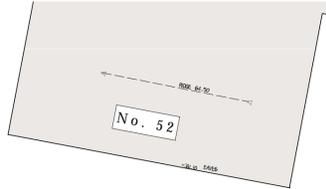
Proposed Development
at
17 CHURCH ROAD
HORLEY RH6 7EY
for Nutfield Homes Ltd

PROPOSED
ELEVATIONS

scale 1:100 1:50 02/02/2021

Rev C 25/07/2021
Rev B 07/02/2021
Rev A 06/02/2021

17CR P4 (C)



STREET SCENE



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Proposed Development
 at
 17 CHURCH ROAD
 HORLEY RH6 7EY
 for Nutfield Homes Ltd

SURVEY

scale 1:200 05/09/2020

17CR P1

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Agenda Item 9

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21/00924/HHOLD

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE	
	DATE:	01 st September 2021	
	REPORT OF:	HEAD OF PLACES AND PLANNING	
	AUTHOR:	Matthew Holdsworth	
	TELEPHONE:	01737 276752	
	EMAIL:	Matthew.Holdsworth@reigate-banstead.gov.uk	
AGENDA ITEM:	9	WARD:	Reigate

APPLICATION NUMBER:	21/00924/HHOLD	VALID:	22 April 2021
APPLICANT:	Mr & Mrs M Cohen	AGENT:	Ralph Guilor Architects Ltd
LOCATION:	46 RAGLAN ROAD, REIGATE, RH2 0DP		
DESCRIPTION:	Proposed first floor extension		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application is referred to Committee by Councillor Whinney

SUMMARY

The proposed development seeks permission for the erection of first floor extension over the front part of the existing bungalow.

The proposal would be of a contemporary design although amended plans have been received during the course of the application for a more conventional roof pitch. Due to the depth of the neighbouring properties, both of which have a first floor and the blank, flank elevations of those properties, it is considered that the relationship with the neighbouring properties is such that no adverse harm would occur as a result of the proposed development and the character of the local area would be respected.

Whilst the existing bungalow is large, the erection of a first floor addition would not appear out of keeping with the height and scale of neighbouring properties and cannot reasonably be resisted.

It is considered that the previous reason for refusal relating to the design of the extension has been overcome. The proposal is therefore considered acceptable.

RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

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Representations:

Letters were sent to neighbouring properties on 30 April 2021, and 05 July 2021. Representations have been received from seven neighbouring properties have been received raising the following concerns:

Issue	Number	Response
Overlooking and loss of privacy	5	See paragraph 6.9 – 6.10
Overshadowing	6	See paragraph 6.8
Poor Design	4	See paragraph 6.3-6.7
Potential conversion into flats	2	See paragraph 6.12
Overbearing	6	See paragraph 6.9 – 6.10
Concern regarding existing building and its position compared to approved plans	2	See paragraph 6.13

1.0 Site and Character Appraisal

- 1.1 The dwelling is a recently constructed bungalow of a contemporary flat roofed design and set within a relatively substantial plot.
- 1.2 The surrounding area is typified by residential development and detached properties of varying sizes and scales and these properties tend to be more traditional in nature with pitched roofs and traditional materials. The site slopes down towards the rear of the garden and the properties to the west are higher than the property. Conversely, the property to the east is set at a slightly lower level. There are no significant trees that would be affected, by the development itself although there is a substantial tree in the front garden and a mature hedge along much of the boundary with Raglan Road.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The opportunity did not arise because the applicant did not approach the Local Planning Authority before submitting the application.
- 2.3 Improvements secured at application stage: Amended plans have been secured improving the design of the proposed extension and the design of the roof.
- 2.2 Further improvements could be secured: Materials as specified, no use of roof as balcony.

3.0 Relevant Planning and Enforcement History

- 3.1 17/02630/F - Proposed replacement single storey dwelling – approved with conditions

- 3.2 20/02395/HHOLD – Proposed first floor extension – refused for the following reason:
The proposed first floor extension, due to its design including high eaves and an expansive flat roof would be out of character with the surrounding area, and would create an incongruous addition in the street scene contrary to policy DES1 of the Reigate and Banstead Development Management Plan 2019 and the SPG on householder extensions and alterations 2004.

4.0 Proposal and Design Approach

- 4.1 The application is for a first floor extension to the front of the property. It would be across the whole width of the property and would be around 6.5m deep.
- 4.2 The proposal seeks to overcome the previous reason for refusal and following amended plans has a conventional roof style and eaves that are more in common with neighbouring properties.
- 4.3 National guidance states that a design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
- Assessment;
 - Involvement;
 - Evaluation; and
 - Design.
- 4.4 Evidence of the applicant’s design approach is set out below:

Assessment	The statement does not include an assessment of local character
	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The statement does not explain why the proposal was chosen

5.0 Policy Context

5.1 Designation

Urban Area

5.2 Reigate and Banstead Core Strategy

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21/00924/HHOLD

CS4 (Valued Townscapes and Historic Environment)

5.3 Reigate & Banstead Development Management Plan 2019

Design DES1

5.4 Other Material Considerations

National Planning Policy Framework

Supplementary Planning Guidance Householder Extensions and Alterations

Other Human Rights Act 1998

6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development.

6.2 The main issues to consider are:

- Design and character
- Neighbour amenity
- Other matters

Design and character

6.3 There is a mixture of a designs of properties in the immediate locality and these are made up of both bungalows and two storey houses, usually of a traditional design that shares design cues with the "Surrey Vernacular". There is no objection in principle to a first floor extension to this property and this was considered in the previous refusal which did not find harm in the provision of a first floor in this location.

6.4 Following amended plans, the proposed roof has a similar eaves height to the property to the east and the ridge height has been reduced to be commensurate with the neighbouring properties. The gable features to the front and rear are acceptable in their design. It is important to note that the proposal is for a first floor extension of around 4m in depth which is less deep than the two houses either side and would represent only around 1/3 of the footprint of the ground floor.

6.5 Development Management Plan (DMP) policy DES1 states:
All new development will be expected to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. Planning permission will be granted for new development where it meets the following criteria: 1. Promotes and reinforces local distinctiveness and respects the character of the surrounding area, including positive physical characteristics of local neighbourhoods and the visual appearance of the immediate street scene.

- 6.6 In addition, in the explanation of the policy, the DMP states:
“Development should address the character and appearance of its surroundings in relation to the immediate vicinity and also the broad locality within which a site is located... Innovation and originality in design will be supported where appropriate visual reference is made to the locality and where local amenity is respected.”
- 6.7 The revisions to the scheme, including the traditional style pitched roof with lowered eaves to that previously refused, along with the gable feature to the front are characteristic features of the area and are considered to have overcome the reason for refusal. Therefore, the proposal complies with policy DES1 in this regard.

Neighbour amenity

- 6.8 The proposed extension would increase the bulk of the property at first floor level. It is important to note that the first floor would only be 6.4m in depth from the front of the building and would cover approximately 1/3 of the total roof space. Objections have been received from both the neighbouring properties at nos. 44 and 48 Raglan Road in terms of visual dominance and loss of light. No.44 is a chalet bungalow and has a largely blank elevation facing the proposed extension along with a hipped gable roof and it is not considered that significant or meaningful overshadowing to that property would occur. Turning to no.48, this is a two storey property slightly set away from the shared boundary with a detached garage adjacent to the boundary. There are some side facing windows at first floor level on this property. However, they appear to be obscure glazed or serve a stairwell. In addition, due to the depth of the first floor extension being approximately 6.4m, this would ensure there would be no material overshadowing to the rear side facing windows of that property. In addition, the eaves height has been reduced from the previous application and the roof has been hipped away, further minimising any significant material impact.
- 6.9 Turning to impacts on privacy on both the neighbouring properties, concern has been raised in terms of overlooking from the rear windows, and especially into the three rooflights of the single storey extension to the rear and side of no.48. The nearest window to no.48 would be for a shower room which will by its nature be obscure glazed. The main concern raised is from the floor to ceiling first floor windows to the rear in the middle of the proposed extension. It is considered that the angle to overlook either adjacent property from these windows, (including into the rooflights) would be oblique and would not cause significant or material overlooking that would be harmful to either adjoining property. Indeed, any overlooking from such a relationship would be highly typical of any normal urban or suburban situation.
- 6.10 If the application were to be approved, a condition would be added to ensure that the remaining flat roof of the property was not used as a balcony to ensure that the neighbouring properties were not overlooked. It is also important to

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note that the main first floor windows in the centre would open out onto the existing internal courtyard and access is not possible from those windows.

- 6.11 Turning to other properties in the locality, it is not considered that the flats to the south on Alma Road would be materially affected due to the distances involved. There has been concern that the properties on the other side of Raglan Road would be overlooked from the front facing first floor windows; however, properties on both sides of the road are set back from the road and some overlooking from front facing windows would be expected in an urban environment.
- 6.12 Consequently, whilst giving rise to a degree of change in the relationship between buildings, the proposed scheme would not adversely affect the amenity of neighbouring properties, and complies in this regard with policy DES1.

Other matters

- 6.13 Concern has been raised from local residents that if the proposal were to be successful, then the property could be divided into a number of units. It is important to note that even if the application were successful, a further planning application would be required to sub-divide the dwelling into smaller units and there is no reason to suspect that this is the intention.
- 6.14 In addition, there has been concern that the existing dwelling was not built in accordance with the approved plans under 17/02630/F. This has been passed onto the planning enforcement team who have determined that the existing dwelling has been built according to the approved plans.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

Plan Type	Reference	Version	Date Received
Location Plan	UNNUMBERED		07.04.2021
Block Plan	UNNUMBERED		07.04.2021

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Floor Plan	01		22.04.2021
Elevation Plan	02		22.04.2021
Elevation Plan	03		22.04.2021
Combined Plan	102	A	02.07.2021

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004

3. The materials to be used in the construction of the external surfaces of the extension shall be those specified in the application.

Reason: To ensure that the development hereby permitted is only constructed using the appropriate external facing materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Development Management Plan policy DES1.

4. The remaining flat roof area of the existing dwelling shall not be used as a balcony, roof garden or similar amenity area without the grant of further specific permission from the Local Planning Authority

Reason: To ensure that the development does not affect the amenity of the neighbouring properties by overlooking with regard to Reigate and Banstead Development Management plan 2019 policy DES1.

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no additional first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed and the windows shall be as per the approved plans.

Reason: To ensure that the development does not affect the amenity of existing properties by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.

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Further information can be found on the Council website at : Climate Change Information.

3. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and
 - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

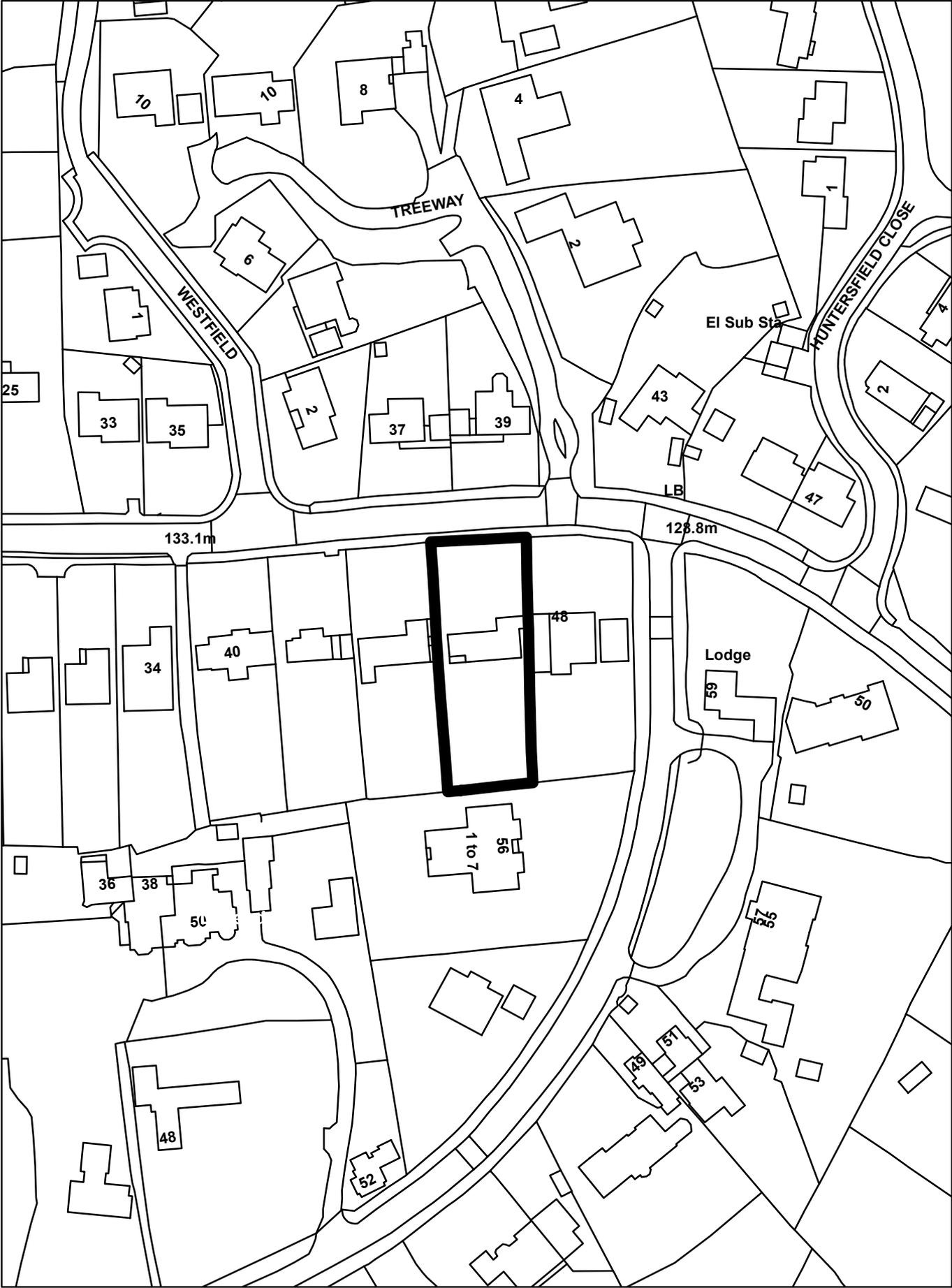
REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policy DES1, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

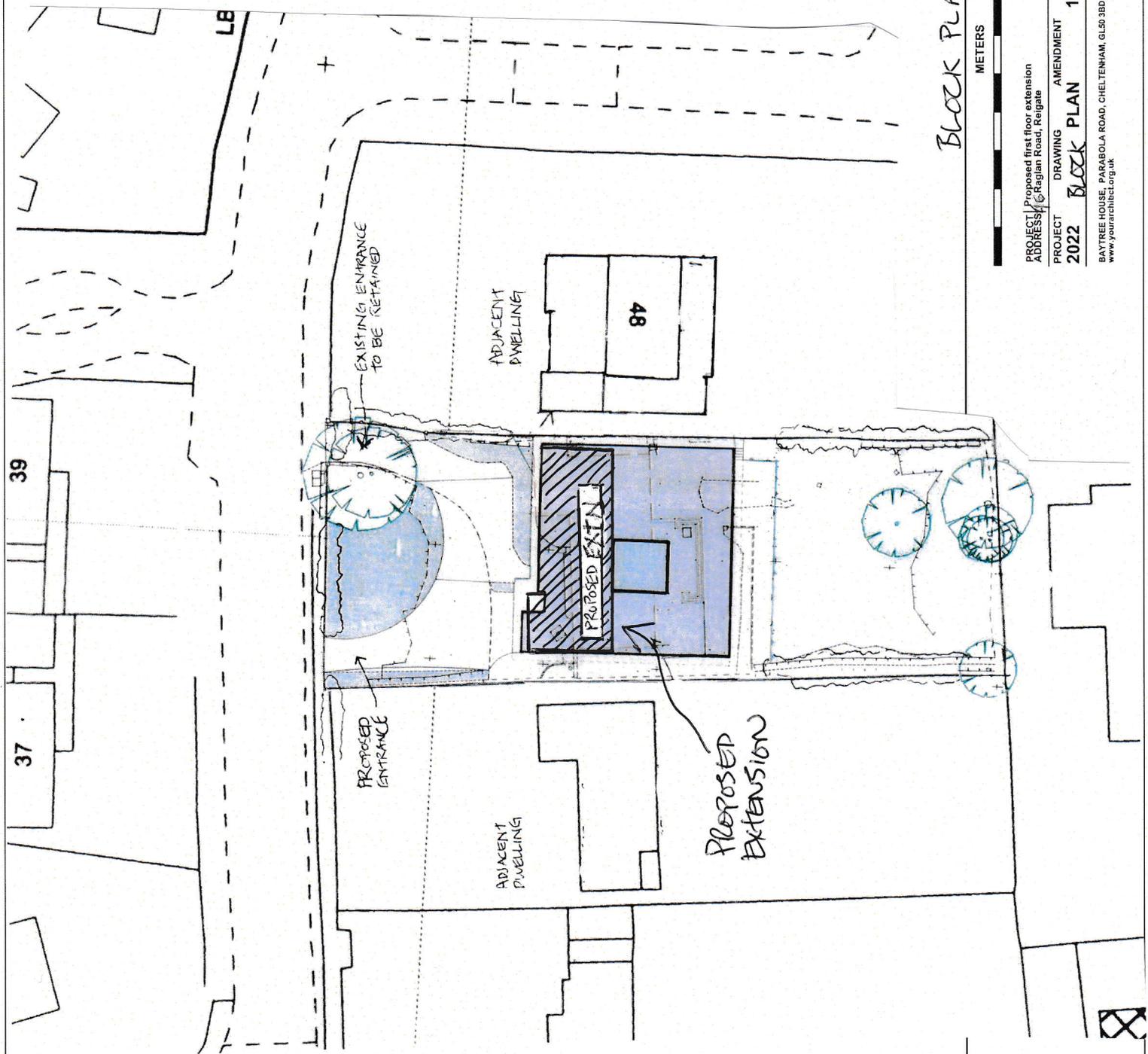
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

21/00924/HHOLD

46 Raglan Road Reigate Surrey RH2 0DP



Agenda Item 9



R
RALPH
GUILOR
ARCHITECTS

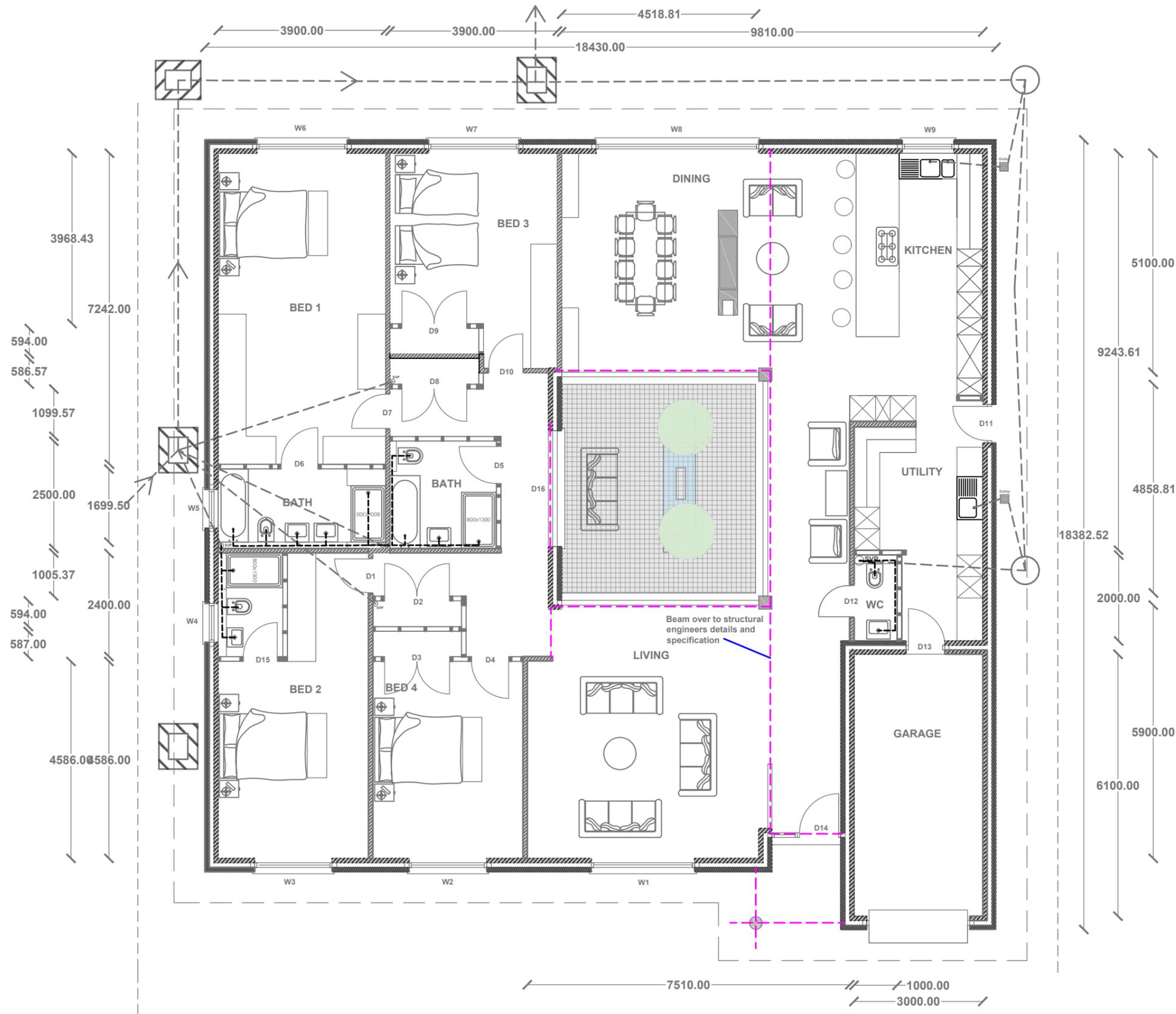
PROJECT | Proposed first floor extension
ADDRESS | Raglan Road, Reigate

PROJECT 2022 DRAWING BLOCK PLAN AMENDMENT SCALE 1:500@A4 DATE Oct 20

BAYTREE HOUSE, PARABOLA ROAD, CHELTENHAM, GL50 3BD
01245 260922, 0772928454
ralph@yournorthernlight.co.uk

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METERS



PROJECT ADDRESS | 46 Raglan Road Reigate- existing plan

PROJECT	DRAWING	AMENDMENT	SCALE	DATE
1706	01		1:100@A3	April 17

BAYTREE HOUSE, PARABOLA ROAD, CHELTENHAM, GL50 3BD 01242 260922, 07792904746
 RALPH@YOURARCHITECT.ORG.UK WWW.YOURARCHITECT.ORG.UK





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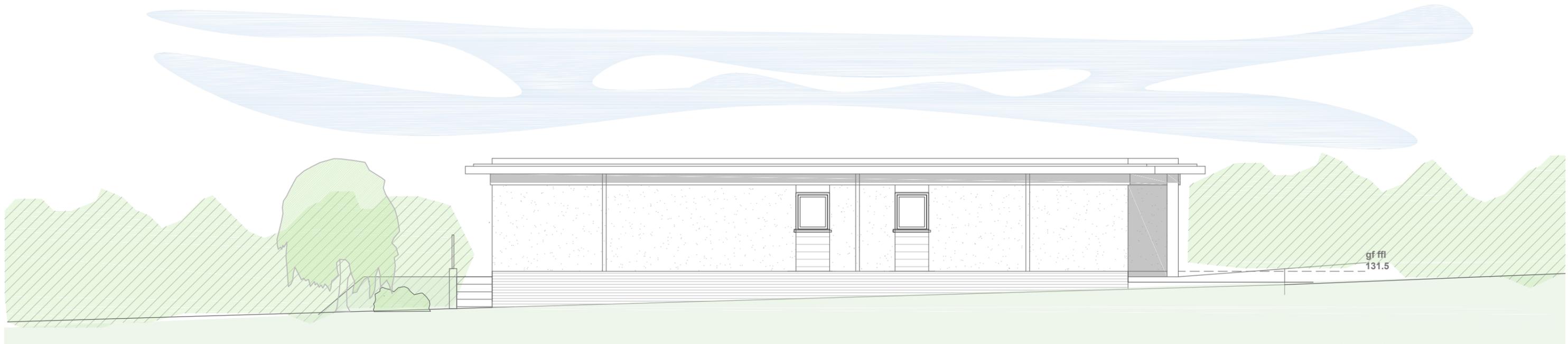


PROJECT ADDRESS | 46 Raglan Road Reigate - existing elevation and section

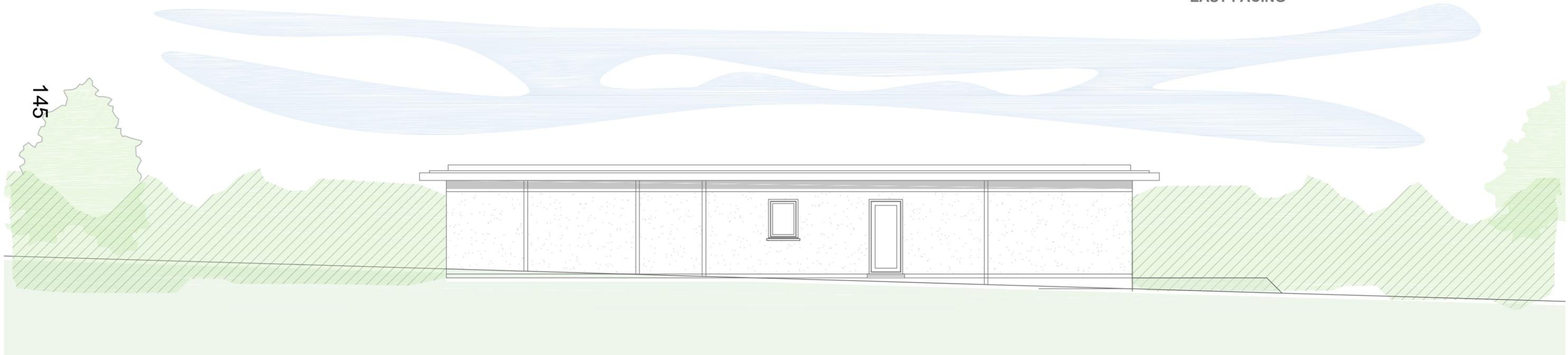
PROJECT	DRAWING	AMENDMENT	SCALE	DATE
1706	O2		1:100@A3	April 17

BAYTREE HOUSE, PARABOLA ROAD, CHELTENHAM, GL50 3BD 01242 260922, 07792904746
 RALPH@YOURARCHITECT.ORG.UK WWW.YOURARCHITECT.ORG.UK





EAST FACING



WEST FACING

AS EXISTING

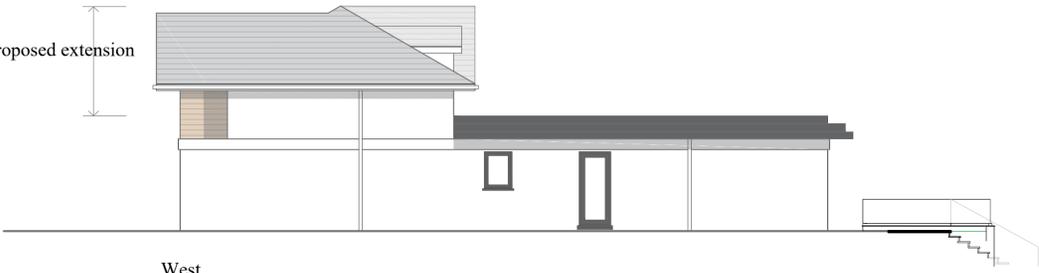
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METERS

PROJECT ADDRESS	46 Raglan Road Horley - existing elevation east, west		
PROJECT	DRAWING	AMENDMENT	SCALE DATE
1706	03		1:100@A3 April 17

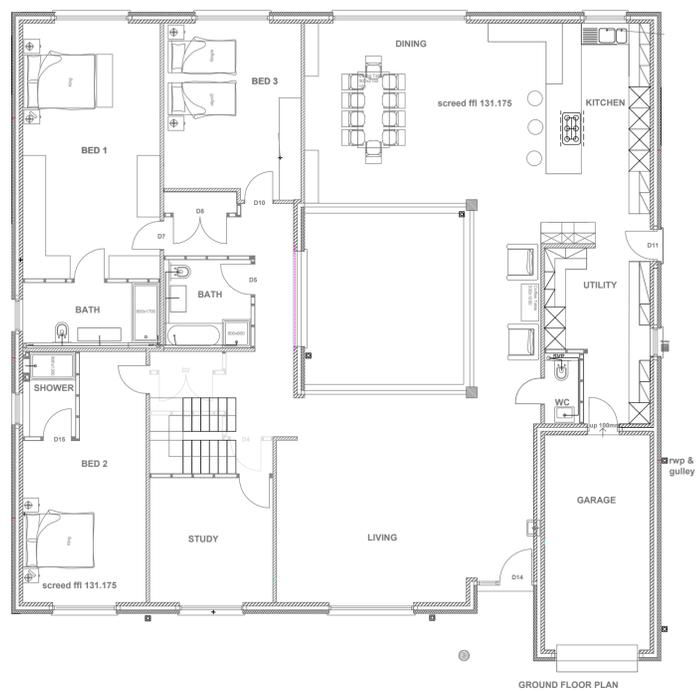
BAYTREE HOUSE, PARABOLA ROAD, CHELTENHAM, GL50 3BD 01242 260922, 07792904746
 RALPH@YOURARCHITECT.ORG.UK WWW.YOURARCHITECT.ORG.UK



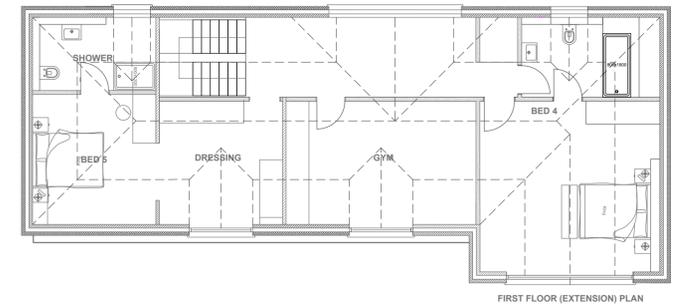
West



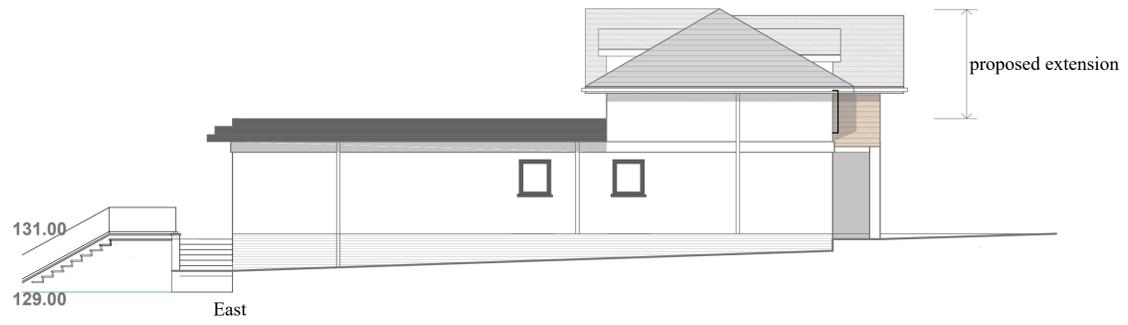
South (through courtyard)



GROUND FLOOR PLAN



FIRST FLOOR (EXTENSION) PLAN



East



EXISTING



South (rear elevation)



PROPOSED



North (front elevation)

146

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METERS

PROJECT Proposed first floor extension plans and elevations
ADDRESS 46 Raglan Road Reigate

PROJECT 2022 DRAWING 102 AMENDMENT A SCALE 1:100@A1 DATE Jun 2021

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Agenda Item 10

Planning Committee
1st September 2021

Agenda Item: 10
DM Performance Q1 2021/22

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE	
	DATE:	1 st September 2021	
	REPORT OF:	HEAD OF PLANNING	
	AUTHORS:	Andrew Benson	
	TELEPHONE:	01737 276175	
	EMAIL:	Andrew.benson@reigate-banstead.gov.uk	
AGENDA ITEM:	10	WARD:	All

SUBJECT:	DEVELOPMENT MANAGEMENT Q1 2021-22 PERFORMANCE
PURPOSE OF REPORT:	To inform members of the 2021/22 Q1 Development Management performance against a range of indicators
RECOMMENDATION:	To note the performance of Q1 2021/22

Planning Committee has authority to note the above recommendation

BACKGROUND

1. Development Management encompasses a wide range of planning activities including pre-application negotiations and engagement; decision making on planning applications through to compliance and enforcement.
2. It puts the Council's locally adopted development plan policies into action and seeks to achieve sustainable development.
3. It is a non-political, legislative system with all Development Management functions falling under the responsibility of the Planning Committee in the Council's Constitution. As such it is a non-Executive function falling outside the scope of the quarterly corporate performance reports that are presented to the Executive and Overview and Scrutiny Committee.
4. Development Management performance has always been monitored and reviewed in line with statutory and local targets with quarterly reports sent to the Department for Communities and Local Government. However, given that all functions of the Council as Local Planning Authority fall under the responsibility of the Planning Committee, the performance information has also been shared with the Planning Committee Chairman. This report enables the performance indicators to be noted by the Planning Committee itself.
5. This report is the first quarterly report of the 2021/22 municipal year and provides the quarterly and end-of-year performance at Table 1. Also provided at Table 2 is the requested performance measure, relating to the time taken in total days from receipt of a valid application to its registration.

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DM Performance Q1 2021/22

PERFORMANCE

	Applications determined (in 8/13 weeks or agreed)	Target	Q1	Q2	Q3	Q4	20/21	Q1
1	Major applications	60%	100%	86%	75%	60%	91%	75%
2	Non-major applications	70%	79%	86%	84%	95%	87%	80%
3	Average days to decision	73	79	88	86	71	80	80
Appeals								
4	Appeals Received	-	13	18	21	22	74	14
5	Major Appeals Decided	-	1	0	1	1	3	1
6	Major Appeals Dismissed	70%	1 (100%)	NA	1 (100%)	1 (100%)	3 (100%)	1 (100%)
7	Non-major appeals Decided	-	5	16	19	16	56	10
8	Non-major appeals Dismissed	70%	4 (80%)	14 (87%)	9 (47%)	10 (62.5%)	37 (66%)	9 (90%)
Enforcement								
9	Reported Breaches		87	124	99	105	417	105
10	Cases Closed		47	107	118	106	374	84
11	On hand at end of period		171	161	149	162	162	190
12	Cases over 6 months old		42	57	58	56	56	62
13	Priority 1 Enforcement	100%	100%	100%	100%	100%	100%	100%
Application Workload								
14	Received		267	319	404	431	1417	478
15	Determined		303	255	330	389	1276	421
16	On hand at end of period		328	379	423	460	460	497
17	Withdrawn		8	8	23	11	50	15

Table 1 - Development Management performance

Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
4.1	5.5	7.1	6.6	7.6	7.1	9	10.2	8.5	9.5	8.1	7.1	11.1	12.1	12.3

Table 2 – Time taken from receipt to registration (working days)

Reason for delay	Number
Awaiting compliance check	10
Awaiting submission of application	11
Awaiting outcome of application	12
Written in past month chasing information/regularisation	8
Open/ongoing prosecution	1
Awaiting Appeal	10
Expediency of harm be concluded with input from statutory consultees	2
Regularising works commenced but not yet complete	3
Chasing up of costs	2
Temporary Stop Notice Served	1
Awaiting planting of replacement tree	1
Delayed by probate	1

Table 3 – Reason for enforcement investigation over 6 months

Planning applications

- 478 planning applications were received in Q1 which builds significantly on the previously high figure reported in Q4 and is the highest number of applications

submitted in any quarter for a number of years. As a result the planning team has been extremely busy with officer caseloads high. The situation is common across the south-east and a number of planning teams are struggling with backlogs in application registrations and determinations. Whilst this has impacted performance to a degree, it has been managed effectively so far. However, it is unlikely to be sustainable and amended working practices and additional resources are being considered.

7. The Town and Country Planning Development Management Procedure Order 2015 sets the statutory period for the determination of planning applications at 8 weeks for non-major applications and 13 weeks for major applications (10+ dwellings or 1,000+ sqm floorspace). This statutory period is relaxed where an extension of time is agreed between the applicant and local planning authority. In order to monitor the performance of local planning authorities, the Government sets targets for the determination of major and non-major planning applications within the statutory period or agreed extension of time. For major developments, this target is 60% and for non-major developments it is 70%.
8. In this Quarter 75% of major applications (6 out of 8) were determined within the statutory period or within agreed extension of time. For non-major applications the figure was 80% for the quarter and so the target was met for both.
9. The average days to decision for Q1 was 80 days, so missing the target of 73 days which is likely to be largely reflective of the high caseloads officers are dealing with.

Planning appeals

10. 14 appeals have been received in the quarter.
11. Alongside the Government performance measure based on speed of determination of planning applications, is the other performance criteria set for local planning authorities aimed at assessing the 'quality' of decision making. This is measured as a percentage of total applications which result in an appeal allowed, broken down between major and non-major development proposals. The relevant target for both types of application is that not more than 10% of applications should be allowed at appeal.
For example –
If 100 major applications are determined by the authority over the qualifying two-year period and 9 are allowed at appeal that would result in a figure of 9% which is acceptable. However, if 100 major applications were determined and 11 of these ended up being appealed and the appeals allowed, this would result in a figure of 11% which fails the 10% target.

The assessment considers appeals allowed against applications refused by each authority across a two year period. Over this latest two-year period 73 major applications were determined meaning 8 or more appeals allowed in the two year period to 31st December 2020 will lead to the target being missed and likely poorly performing designation together with the loss of control by virtue of the ability to submit applications directly to the Secretary of State.

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DM Performance Q1 2021/22

12. In this last quarter one major appeal was determined and it was dismissed. However, only a few major appeals being allowed can make a significant impact and so it is still a target to be aware of.
13. 9 out of the 10 non-major appeals determined in this quarter were dismissed representing 90% dismissed which exceeds the 70% target but of course this will continue to be monitored across the year to ensure this appeal statistic is on target.

Planning Enforcement

14. There were 105 reported enforcement breaches in the quarter, continuing the high numbers reported last year. This is common across the County and probably nationally as was reported in the national planning press in December. It is likely to be a result of the combination of more people being locked down at home, spending more time observing development in their neighborhoods as well as some changes resulting from Government changes in response to Covid such as extending construction hours. When combined with the backlog associated with the difficulty in closing some cases whilst restrictions in site inspections were in place, and the sickness absence of one enforcement officer, this has resulted in an increase in the number of enforcement cases on hand and over 6 months across the year.

Registration

15. Table 2 shows that performance in the time taken from receipt to registration of new applications. This has remained high following June last year when application submissions increased in combination with a reduction in staff (now being addressed) and some constraints associated with remote working given the need for fast IT and postal notifications with this work. This quarter has seen the longest time taken to register applications for a significant period of time, associated with the high number of submissions and a vacant post. Now that the team is fully staffed, the backlog has been overcome and a better picture will be reported for the next quarter.

Overall

16. Performance has held up despite record numbers of planning application submissions and reported enforcement breaches which are causing many planning departments locally to experience validation and registration delays of a month or more and consequent delays to the determination period for applications, with backlogs building. Planning Officers across the south-east are struggling with high caseloads. Good temporary staff are in high demand and so not an automatic solution to the issue but will continue to be considered and permanent recruitment is difficult with so much uncertainty over planning currently. Other options such as suspension of discretionary services or changing working practices may need to be considered if workloads become unmanageable as have been undertaken elsewhere although these are not currently being recommended.